THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Anthony Lee Washington III
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HDR ENGINEERING, INC.
120 BRENTWOOD COMMONS WAY
SUITE 525
BRENTWOOD, TN 37027
ANTHONY L. WASHINGTON III P.E. 119749

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE OF TENN. CODE ANN. §62-2-306.

SHEET NAME	SHEET NO.
SIGNATURE SHEET	ROADWAY-SIGN1
TITLE SHEET	1
ROADWAY INDEX, STANDARD ROADWAY DRAWINGS AND STANDARD TRAFFIC DESIGN DRAWINGS	1A
PROJECT COMMITMENTS	1B
ESTIMATED ROADWAY QUANTITIES	2
TYPICAL SECTIONS AND PAVEMENT SCHEDULE	2B
GENERAL NOTES	2C
SPECIAL NOTES	2D
ENVIRONMENTAL NOTES	2E, 2E1
TABULATED QUANTITIES	2F
UTILITY NOTES AND UTILITY OWNERS	3
PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL	4

YEAR	PROJECT NO.	SHEET NO.
2025	STP/HSIP-57(93)	ROADWAY-SIGN1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE SHEET

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Index Of Sheets
SEE SHEET NO. 1A

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BUREAU OF ENGINEERING

FAYETTE COUNTY

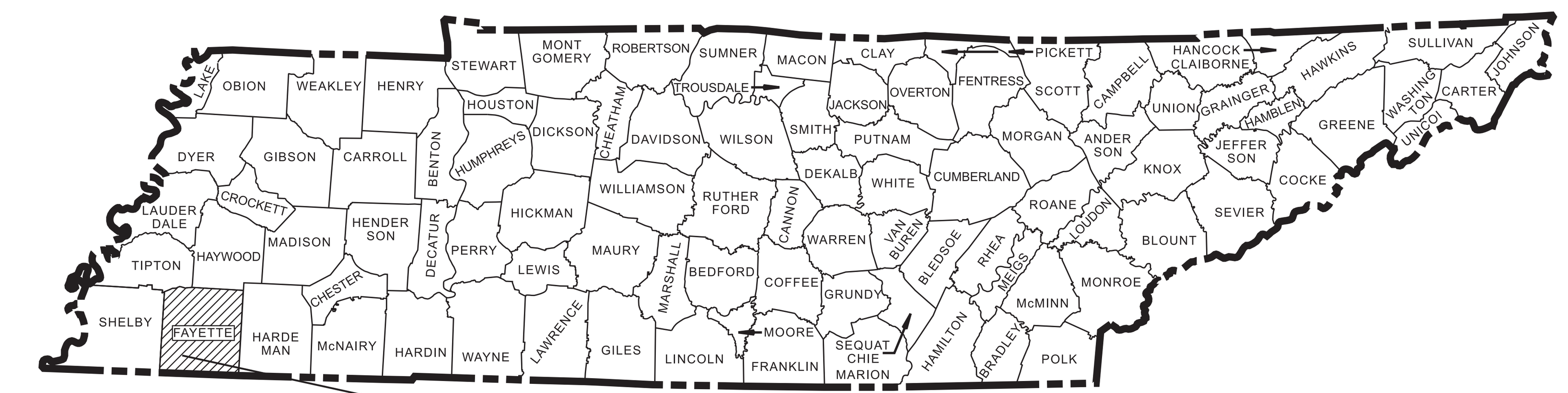
SR-57
FROM: L.M. 5.72 (SR-194)
TO: L.M. 20.14 (COWAN LOOP)

RESURFACE & SAFETY
SCRUB SEAL, TLD, GUARDRAIL, AND PAVEMENT MARKINGS

STATE HIGHWAY NO. 57 F.A.H.S. NO. N/A

DOES THIS PROJECT QUALIFY FOR UTILITY CHAPTER 86	YES	NO X
WORK ZONE SIGNIFICANCE DETERMINATION		
SIGNIFICANT	YES	NO X

TENN.	YEAR	SHEET NO.
	2025	1
FED. AID PROJ. NO.	STP/HSIP-57(93)	
STATE PROJ. NO.	24S057-F8-004	
STATE PROJ. NO.	24S057-F3-004	
STATE PROJ. NO.	24S057-M3-002	



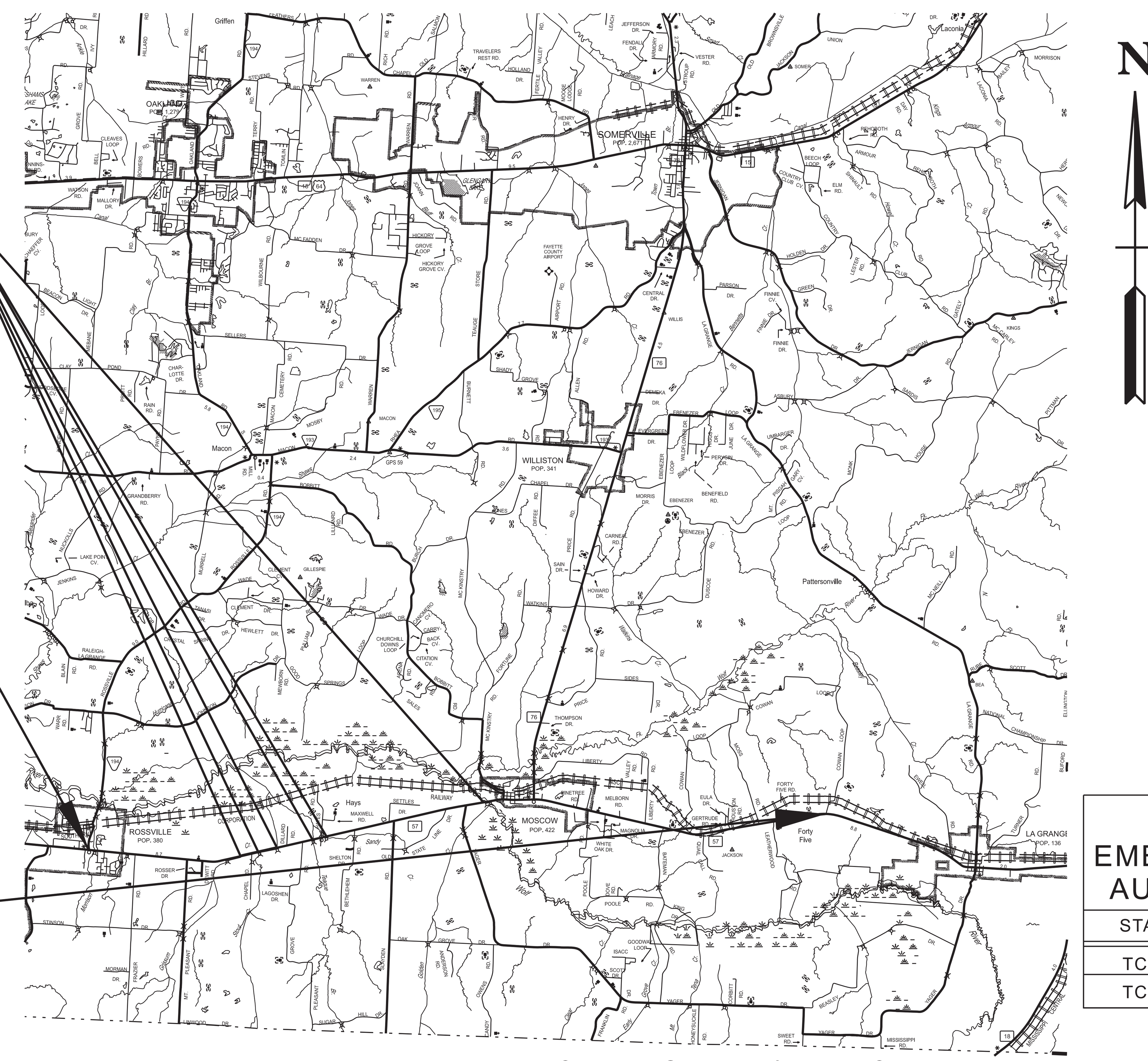
PROJECT LOCATION

BRIDGE ID. # 24SR0570011 24SR0570013 24SR0570015 24SR0570017 24SR0570019 24SR0570021 24SR0570023 24SR0570025

NO EXCLUSIONS

BRIDGE DECK REPAIR PROJECT NO. 24S057-M3-002
SR-57 L.M. 8.66
SR-57 L.M. 9.00
SR-57 L.M. 9.36
SR-57 L.M. 10.06
SR-57 L.M. 10.34
SR-57 L.M. 13.44

24S057-F8-004
24S057-F3-004
BEGIN PROJECT NO. STP/HSIP-57(93) RESURFACE
L.M. 5.72 (SR-194)



24S057-F8-004
24S057-F3-004
END PROJECT NO. STP/HSIP-57(93) RESURFACE
L.M. 20.14 (COWAN LOOP)

SCALE: 1" = 10560'
0 2 4 6 MILES

PROJECT LENGTH 14.42 MILES
TOTAL LANE MILES RESURFACED 28.84 MILES

SPECIAL NOTES

PROPOSALS MAY BE REJECTED BY THE COMMISSIONER IF ANY OF THE UNIT PRICES CONTAINED THEREIN ARE OBVIOUSLY UNBALANCED, EITHER EXCESSIVE OR BELOW THE REASONABLE COST ANALYSIS VALUE.

THIS PROJECT TO BE CONSTRUCTED UNDER THE STANDARD SPECIFICATIONS OF THE TENNESSEE DEPARTMENT OF TRANSPORTATION DATED JANUARY 1, 2021 AND ADDITIONAL SPECIFICATIONS AND SPECIAL PROVISIONS CONTAINED IN THE PLANS AND IN THE PROPOSAL CONTRACT.

TDOT PROJECT MANAGER : LYNN EVANS, P.E., REG. 4
DESIGNED BY : HDR ENGINEERING, INC.
DESIGNER : ANTHONY L. WASHINGTON, III, P.E. CHECKED BY DAVID HORNE, P.E.
P.E. NO. 98043-4283-04
PIN NO. 132486.00

TRAFFIC COUNTER, EMBEDDED DETECTION LOOPS & AUTOMATIC TRAFFIC READERS	
STATION LOCATION	LOG MILE
TC STATION 64	6.198
TC STATION 56	14.725

TRAFFIC DATA	
ADT (2025)	5,315

POSTED SPEED LIMITS	
L.M. 5.72 TO L.M. 6.45	45 MPH
L.M. 6.45 TO L.M. 13.74	55 MPH
L.M. 13.74 TO L.M. 15.87	40 MPH
L.M. 15.87 TO L.M. 20.14	55 MPH

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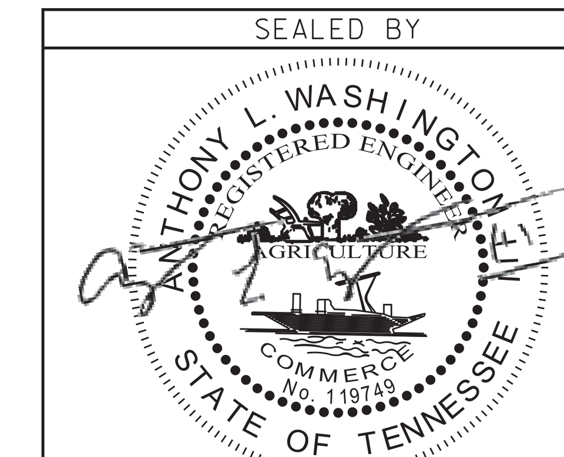

2/25/2025

APPROVED: WILL REID, CHIEF ENGINEER
DATE:
APPROVED: HOWARD H. ELEY, COMMISSIONER

U.S. DEPARTMENT OF TRANSPORTATION
FEDERAL HIGHWAY ADMINISTRATION

APPROVED:
DIVISION ADMINISTRATOR DATE

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	STP/HSIP-57(93)	1A

ROADWAY INDEX

NO UTILITY SHEETS INCLUDED IN THIS SET OF PLANS.

STANDARD ROADWAY DRAWINGS

DWG.	REV.	DESCRIPTION
STANDARD ROADWAY TITLE SHEET, ABBREVIATIONS, AND LEGENDS		
RD-A-1	02-20-20	STANDARD ABBREVIATIONS A THROUGH L
RD-A-2		STANDARD ABBREVIATIONS M THROUGH Z
RD-L-1	02-20-20	STANDARD LEGEND
RD-L-1A		STANDARD LEGEND
SAFETY DESIGN AND GUARDRAILS		
S-GR31-1	06-15-21	GUARDRAIL DETAILS
S-GR31-1A	06-28-19	GUARDRAIL AND BLOCK-OUT DETAILS
S-GR31-1B		GUARDRAIL FASTENING HARDWARE
S-GR31-1C	07-07-23	GUARDRAIL GENERAL NOTES AND POST DETAILS
S-GRS-4	05-04-22	SPECIAL CASE GUARDRAIL HEIGHT TRANSITION DETAIL
S-GRT-2	06-28-19	TYPE 38 GUARDRAIL END TERMINAL
S-GRT-2R	06-28-19	EARTH PAD FOR TYPE 38 AND TYPE 21 TERMINAL (RETROFIT)

STANDARD TRAFFIC DESIGN DRAWINGS

DWG.	REV.	DESCRIPTION
DESIGN - TRAFFIC CONTROL		
T-M-1	01-24-25	DETAILS OF PAVEMENT MARKINGS FOR CONVENTIONAL ROADS AND MARKING ABBREVIATIONS
T-M-3	01-24-25	MARKING STANDARDS FOR TRAFFIC ISLANDS, PAVED SHOULDERS AND MEDIANS FOR CONVENTIONAL ROADS
T-M-4	01-24-25	STANDARD INTERSECTION PAVEMENT MARKINGS
T-M-16	01-24-25	RUMBLE STRIPE INSTALLATION LAYOUT
T-M-16A	01-24-25	RUMBLE STRIPE DETAILS FOR EDGE OF PAVEMENT AND CENTERLINE
T-WZ-10	04-02-12	ADVANCE ROAD WORK SIGNING ON HIGHWAYS AND FREEWAYS

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2/25/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ROADWAY INDEX, STANDARD ROADWAY DRAWINGS AND STANDARD TRAFFIC DESIGN DRAWINGS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	STP/HSIP-57(93)	1B

PROJECT COMMITMENTS

COMMITMENT ID	SOURCE DIVISON	DESCRIPTION	STA. / LOCATION
EDHZ001	ENVIRONMENTAL DIVISION HAZARDOUS MATERAILS	ASBESTOS CONTAINING MATERIAL (ACM) SURVEYS WERE COMPLETED ON THE FOLLOWING BRIDGES AND NO ASBESTOS WAS DETECTED. PLEASE SEE THE REPORTS FOR FURTHER DETAILS AND PHOTOGRAPHS. BRIDGE NO. 24SR0570015 SR-57 OVER GRISSUM CREEK LM 8.66 (24-SR057-08.66) BRIDGE NO. 24SR0570017 SR-57 OVER OVERFLOW LM 9.00 (24-SR057-09.00) BRIDGE NO. 24SR0570019 SR-57 OVER STOUT CREEK LM 9.36 (24-SR057-09.36) BRIDGE NO. 24SR0570021 SR-57 OVER TEAGUE BRANCH LM 10.06 (24-SR057-10.06) BRIDGE NO. 24SR0570023 SR-57 OVER SANDY CREEK LM 10.34 (24-SR057-10.34) BRIDGE NO. 24SR0570025 SR-57 OVER WOLF RIVER LM 13.44 (24-SR057-13.44)	BRIDGE NO. 24SR0570015 SR-57 OVER GRISSUM CREEK LM 8.66 BRIDGE NO. 24SR0570017 SR-57 OVER OVERFLOW LM 9.00 BRIDGE NO. 24SR0570019 SR-57 OVER STOUT CREEK LM 9.36 BRIDGE NO. 24SR0570021 SR-57 OVER TEAGUE BRANCH LM 10.06 BRIDGE NO. 24SR0570023 SR-57 OVER SANDY CREEK LM 10.34 BRIDGE NO. 24SR0570025 SR-57 OVER WOLF RIVER LM 13.44
EDHZ002	ENVIRONMENTAL DIVISION HAZARDOUS MATERAILS	NO SPECIAL ACCOMMODATIONS FOR DEMOLITION AND WASTE DISPOSAL ARE ANTICIPATED FOR THESE STRUCTURES AND THE MATERIAL CAN BE DEPOSITED IN A C&D LANDFILL. PRIOR TO THE DEMOLITION OR REHABILITATION OF ANY STRUCTURE (BRIDGE OR BUILDING), THE CONTRACTOR IS REQUIRED TO SUBMIT THE NATIONAL EMISSION STANDARDS FOR HAZARDOUS AIR POLLUTANTS STANDARD 10-DAY NOTICE OF DEMOLITION TO THE TDEC DIVISION OF AIR POLLUTION CONTROL (PER TDOT STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION (JANUARY 1, 2021) SECTIONS 107.08.D AND 202.03).	

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PROJECT
COMMITMENTS

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ESTIMATED ROADWAY QUANTITIES

	ITEM NO.	DESCRIPTION	UNIT	QUANTITY 24S057-F8-004	QUANTITY 24S057-F3-004	TOTAL QUANTITY
	202-03.01	REMOVAL OF ASPHALT PAVEMENT	S.Y.	428		428
	203-06	WATER	M.G.	22		22
(1)	208-01.05	BROOMING & DEGRASSING SHOULDERS	L.M.	28.4		28.4
(2)	303-02	MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D)	TON	2863		2863
(3)	307-01.01	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING A	TON	148		148
(3)	307-01.08	ASPHALT CONCRETE MIX (PG64-22) (BPMB-HM) GRADING B-M2	TON	366		366
(4)(5)	403-02.01	TRACKLESS TACK COAT	TON	66		66
(6)	411-03.12	ACS MIX(PG64-22) THIN LIFT D ASPHALT	TON	8944		8944
(7)	411-12.04	SCORING FOR RUMBLE STRIPE (NON-CONTINUOUS) (4IN WIDTH)	L.M.	23		23
(8)	414-04.03	ASPHALT EMULSION (SCRUB SEAL)	TON	267		267
(8)	414-04.04	MINERAL AGGREGATE (SCRUB SEAL)	TON	2204		2204
(9)	415-01.01	COLD PLANING BITUMINOUS PAVEMENT	TON	572		572
(10)	705-02.10	GUARDRAIL TRANSITION 27IN TO 31IN	EACH		15	15
(10)	705-04.09	EARTH PAD FOR TYPE 38 GR END TREATMENT	EACH		15	15
(10)	705-06.10	GR TERMINALTRAILING END (TYPE 13) MASH TL3	EACH		2	2
(10)	705-06.11	GR TERMINAL (IN-INLINE) MASH TL3	EACH		2	2
(10)	705-06.20	TANGENT ENERGY ABSORBING TERM MASH TL-3	EACH		11	11
(10)	705-06.30	GR TERMINAL (ENERGY ABSORBING) MASH TL2	EACH		2	2
(10)	706-01	GUARDRAIL REMOVED	L.F.		700	700
(10)	706-06.03	RADIUS RAIL	L.F.		40	40
(11)	712-01	TRAFFIC CONTROL	LS	1		1
(12)	712-06	SIGNS (CONSTRUCTION)	S.F.	1392		1392
	716-01.21	SNOWPLOWABLE RAISED PAVEMENT MARKERS (BI-DIR) (1 COLOR)	EACH	381	550	931
	716-01.22	SNOWPLOWABLE RAISED PAVMENT MARKERS (MONO-DIR)(1 COLOR)	EACH	18		18
	716-01.30	REMOVAL OF SNOWPLOWABLE REFLECTIVE MARKER	EACH	399		399
(13)(14)	716-02.04	PLASTIC PAVEMENT MARKING(CHANNELIZATION STRIPING)	S.Y.	60	199	259
(13)(14)	716-02.05	PLASTIC PAVEMENT MARKING (STOP LINE)	L.F.	700		700
(13)(14)	716-02.06	PLASTIC PAVEMENT MARKING (TURN LANE ARROW)	EACH	5		5
(13)(14)	716-02.12	PLASTIC PAVEMENT MARKING (8IN LINE)	L.M.		0.1	0.1
(13)(14)	716-03.01	PLASTIC WORD PAVEMENT MARKING (ONLY)	EACH	2		2
(13)(14)	716-04.04	PLASTIC PAVEMENT MARKING (TRANSVERSE SHOULDER)	L.F.	96	978	1074
(13)(14)	716-04.12	PLASTIC PAVEMENT MARKING (YIELD LINE)	S.F.	12		12
(15)	716-05.20	PAINTED PAVEMENT MARKING (6" LINE)	L.M.	46.3		46.3
	716-08.04	REMOVAL OF PAVEMENT MARKING (CHANNELIZATION STRIPING)	S.Y.	60		60
	716-08.06	REMOVAL OF PAVEMENT MARKING (TURN LANE ARROW)	EACH	5		5
	716-08.11	REMOVAL OF WORD PAVEMENT MARKING (ONLY)	EACH	2		2
	716-08.20	REMOVAL OF PAVEMENT MARKING (LINE)	L.M.	46.3		46.3
(13)	716-12.02	ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE)	L.M.		46.3	46.3
	717-01	MOBILIZATION	LS	1		1

FOOTNOTES

(1) TO BE USED AS DIRECTED BY THE ENGINEER. INCLUDES THE COST OF REMOVING DEBRIS AND SWEEPING SHOULDER PRIOR TO WORK. SEE SHEET 2D, PAVEMENT, RESURFACING, NOTE 2 FOR MORE INFORMATION.

(2) TO BE USED AS DIRECTED BY THE ENGINEER.

(3) FOR REPLACEMENT OF QUANTITY REMOVED UNDER ITEM 202-03.01 SEE SHEET 2B.

(4) THE FINAL SCRUB SEAL SURFACE SHALL BE SWEPT IMMEDIATELY BEFORE APPLYING TACK COAT. THIS SWEEPING IS IN ADDITION TO THE SWEEPING REQUIRED UNDER SPECIAL PROVISION 405SS.

(5) INCLUDES 2 TONS FOR DRIVEWAYS, FIELD ENTRANCES, BUSINESS ENTRANCES, COUNTY ROADS, AND EXTRA WIDTH PAVING AND 2 TONS FOR BREAKOUT AREAS.

(6) INCLUDES 215 TONS FOR COUNTY ROADS, DRIVEWAYS, FIELD ENTRANCES, BUSINESS ENTRANCES AND EXTRA WIDTH PAVING AREAS. INCLUDES 100 TONS FOR SPOT LEVELING.

(7) TO BE USED FROM L.M. 5.72 TO L.M. 9.00 AND L.M. 10.9 TO L.M. 14.00. GRIND AT A DEPTH OF 3/8" +/- 1/16". LONGITUDINAL SPACING MAY BE AFFECTED BY SHALLOWER GRINDS. RUMBLE GRINDING SHOULD NOT PENETRATE COMPLETELY THROUGH THE NEWLY PAVED LAYER OR CAUSE PREMATURE DAMAGE.

(8) SCRUB SEAL SHALL EXTEND TO THE INSIDE OF THE RUMBLE STRIPES (WHEN PRESENT). WHEN RUMBLE STRIPES ARE NOT PRESENT, THE SCRUB SEAL SHALL EXTEND TO THE OUTSIDE OF THE SHOULDER. BEFORE PLACING SCRUB SEAL THE CONTRACTOR IS REQUIRED TO REMOVE ANY EXISTING THERMOPLASTIC PAVEMENT MARKINGS THAT ARE TO BE COVERED BY SCRUB SEAL, INCLUDING ALL LANE LINES AND SPECIALTY MARKINGS. THE CONTRACTOR SHALL ONLY REMOVE PAVEMENT MARKINGS THAT ARE TO BE COVERED DURING THE DAY'S CONSTRUCTION ACTIVITIES. THE CONTRACTOR IS TO TAKE EXTRA CARE TO ENSURE THAT THE EXISTING ASPHALT SURFACE IS NOT DAMAGED DURING THERMOPLASTIC REMOVAL. THE ROADWAY MUST BE FREE OF EXCESS DUST OR DEBRIS AS A RESULT OF THERMOPLASTIC REMOVAL BEFORE SCRUB SEAL IS TO BE PLACED.

(9) INCLUDES 296 TONS FOR BREAKOUT AREAS AND 276 TONS FOR TAPER MILLING AT PROJECT LIMITS AND BRIDGE ENDS, SEE SHEET 2B FOR DETAILS.

(10) SEE PROPOSED GUARDRAIL (RESURFACING) SHEET 2F.

(11) THE CONTRACTOR SHALL COMPLY WITH SECTION 712 OF THE STANDARD SPECIFICATIONS FOR ROAD AND BRIDGE CONSTRUCTION REGARDING TEMPORARY TRAFFIC CONTROL AND THE CURRENT EDITION OF THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES. THIS ITEM INCLUDES TRAFFIC CONTROL FOR ANY BRIDGE REPAIR WORK. ALL SIGNING SHALL BE IN ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES (MUTCD).

(12) THE CONTRACTOR IS RESPONSIBLE FOR THE STAKING OF CONSTRUCTION SIGNS. IN THE EVENT THAT A CONSTRUCTION AND/OR REGULATORY SIGN IS TEMPORARILY DESIGNATED NOT IN USE DURING THE CONSTRUCTION PHASE OF A PROJECT, THE CONTRACTOR SHALL CHOOSE A SIGN COVERING APPROVED BY THE ENGINEER. TEMPORARY SIGN COVERINGS SHALL NOT BE PAID FOR DIRECTLY BUT SHALL BE INCLUDED IN THE PRICE BID FOR ITEM NO. 712-06 SIGNS (CONSTRUCTION). SEE TRAFFIC CONTROL SIGN TABULATION (RESURFACING), SHEET 2F.

(13) ITEM TO BE USED FOR FINAL PAVEMENT MARKING ONLY.

(14) THE CONTRACTOR MAY ELECT TO SUBSTITUTE PREFORMED PLASTIC FOR THERMOPLASTIC. PREFORMED PLASTIC SHALL BE PAID FOR AT THE SAME UNIT PRICE AS BID FOR THERMOPLASTIC.

(15) ITEM TO BE USED FOR TEMPORARY PAVEMENT MARKING ONLY.

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	STP/HSIP-57(93)	2

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ESTIMATED ROADWAY QUANTITIES

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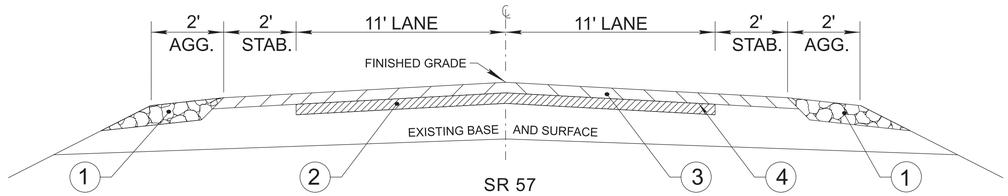
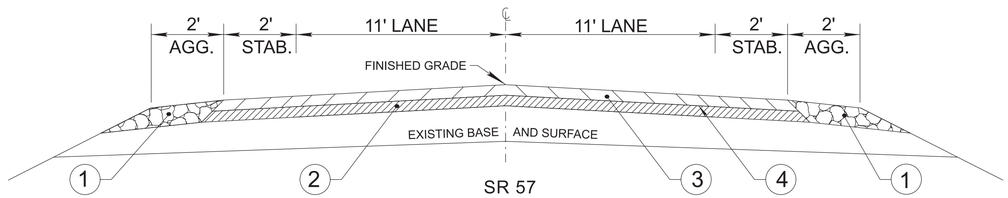
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	STP/HSIP-57(93)	2B



SR 57
TYPICAL SECTION
L.M. 5.72 TO L.M. 9.00
L.M. 10.90 TO L.M. 14.00



SR 57
TYPICAL SECTION
L.M. 9.00 TO L.M. 10.90
L.M. 14.00 TO L.M. 20.14

PROPOSED PAVEMENT SCHEDULE

(1) MINERAL AGGREGATE BASE @ 1.50"± THICK FOR SHOULDERS

303-02 MINERAL AGGREGATE, TYPE B BASE, GRADING (C OR D)

(2) SCRUB SEAL

414-04.03 ASPHALT EMULSION (SCRUB SEAL)
ASPHALT EMULSION (AT 0.25 - 0.35 GAL./S.Y.)

414-04.04 MINERAL AGGREGATE (SCRUB SEAL)
MINERAL AGGREGATE @ (18 - 25 LBS./S.Y.)

(SCRUB SEAL SHALL EXTEND TO THE INSIDE OF THE RUMBLE STRIPES (WHEN PRESENT)

(3) ASPHALTIC CONCRETE SURFACE (ACS) @ 0.80"± THICK (APPROX. 85.00 LBS./S.Y.)

ITEM 411-03.12 ACS MIX (PG64-22) THIN LIFT D ASPHALT

(4) TRACKLESS TACK COAT

ITEM 403-02.01 TRACKLESS TACK COAT (TC)
SEE 403.05 FOR DETERMINING APPLICATION RATE IN THE FIELD.

(5) BITUMINOUS COURSE (BINDER) @ 2.00"± THICK (APPROX. 226.0 LBS./S.Y.)

307-01.08 ASPHALT CONCRETE MIX (PG64-22)(BPMB-HM) GRADING "B-M2"
(TO BE USED FOR BREAKOUT ONLY SEE BREAKOUT LOCATION DETAIL)

(6) BITUMINOUS COURSE (BLACK BASE) @ 6.00"± THICK (APPROX. 690.0 LBS./S.Y.)

307-01.01 ASPHALT CONCRETE MIX (PG64-22)(BPMB-HM) GRADING "A"
(TO BE APPLIED IN 2 EQUAL LIFTS) (TO BE USED FOR BREAKOUT ONLY SEE BREAKOUT LOCATION DETAIL)

REMOVAL OF ASPHALT PAVEMENT

202-03.01 REMOVAL OF ASPHALT PAVEMENT (S.Y.)
(TO BE USED FOR BREAKOUT ONLY SEE BREAKOUT LOCATION DETAIL)

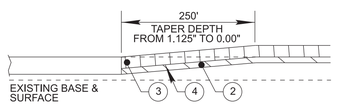
COLD PLANING @ 1.125"± THICK (APPROX. 118.125 LBS/SY)

415-01.01 COLD PLANING BITUMINOUS PAVEMENT (TON)
(TO BE USED FOR 250' TRANSTIONS AT PROJECT LIMITS AND BRIDGE APPROACHES FOR BRIDGES 24SR0570017, 24SR0570019, 24SR0570021, 24SR0570023 & 24SR0570025)

COLD PLANING @ 2.00"± THICK (APPROX. 210.0 LBS/SY)

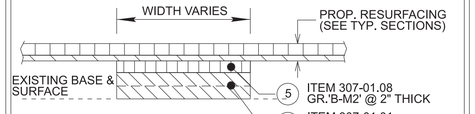
415-01.01 COLD PLANING BITUMINOUS PAVEMENT (TON)
(TO BE USED FOR BREAKOUT ONLY SEE BREAKOUT LOCATION DETAIL)

DETAIL OF TAPER MILL



AT PROJECT LIMITS L.M. 5.72, L.M. 20.14
AT BRIDGE APPROACHES FOR BRIDGES
24SR0570015, 24SR0570017, 24SR0570019, 24SR0570021,
24SR0570023, & 24SR0570025 (NOT TO SCALE)

DETAIL OF BREAKOUT



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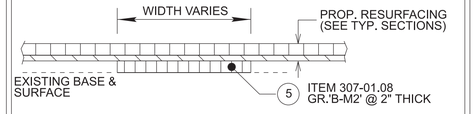
DETAIL OF BREAKOUT



(NOT TO SCALE)

BRIDGE NOTES

LOCATION	BRIDGE NO.	LENGTH (FT.)	TREATMENT
L.M. 5.84	24SR0570011	53.40	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 6.45	24SR0570013	23.00	PAVE WITH PLANS MIX/TREATMENT TYPE
L.M. 8.66	24SR0570015	104.33	SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED)
L.M. 9.00	24SR0570017	104.06	SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED)
L.M. 9.36	24SR0570019	141.08	SEAL WITH TYPE 1 THIN EPOXY OVERLAY & REPLACE BRIDGE JOINTS (BRIDGE SHEETS INCLUDED)
L.M. 10.06	24SR0570021	131.23	SEAL WITH TYPE 1 THIN EPOXY OVERLAY & REPLACE BRIDGE JOINTS (BRIDGE SHEETS INCLUDED)
L.M. 10.34	24SR0570023	131.23	SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED)
L.M. 13.44	24SR0570025	1422.22	SEAL WITH TYPE 1 THIN EPOXY OVERLAY (BRIDGE SHEETS INCLUDED)

BREAKOUT LOCATION DETAIL

LM	DIRECTION	L	W	TREATMENT
8.644	Both EB & WB	50	22	2" MILL & BM-2
8.676	Both EB & WB	50	22	2" MILL & BM-2
8.940	WB	75	11	2" MILL & BM-2
9.018	Both EB & WB	50	22	2" MILL & BM-2
9.278	Both EB & WB	150	22	2" MILL & BM-2
9.380	Both EB & WB	100	22	2" MILL & BM-2
9.430	WB	100	22	2" MILL & BM-2
9.998	EB	100	11	REMOVAL, BM-2 & A-MIX
10.020	EB	150	11	REMOVAL, BM-2 & A-MIX
10.030	WB	100	11	REMOVAL, BM-2 & A-MIX
10.092	Both EB & WB	50	22	2" MILL & BM-2
10.251	Both EB & WB	50	22	2" MILL & BM-2
10.273	WB	75	11	2" MILL & BM-2
10.326	Both EB & WB	100	22	2" MILL & BM-2
10.366	Both EB & WB	100	22	2" MILL & BM-2
10.430	EB	50	11	2" MILL & BM-2
13.710	Both EB & WB	100	22	2" MILL & BM-2
13.727	WB	100	11	2" MILL & BM-2
20.130	WB & EB	100	22	2" MILL & BM-2

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DEPARTMENT OF TRANSPORTATION

TYPICAL SECTIONS AND PAVEMENT SCHEDULE

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	STP/HSIP-57(93)	2C

GENERAL NOTES

GRADING

(1) ANY AREA THAT IS DISTURBED OUTSIDE LIMITS OF CONSTRUCTION DURING THE LIFE OF THIS PROJECT SHALL BE REPAIRED BY THE CONTRACTOR AT HIS EXPENSE.

(3) THE CONTRACTOR SHALL NOT DISPOSE OF ANY MATERIAL EITHER ON OR OFF STATE-OWNED R.O.W. IN A REGULATORY FLOOD WAY AS DEFINED BY THE FEDERAL EMERGENCY MANAGEMENT AGENCY (FEMA) WITHOUT APPROVAL BY FEMA. ALL MATERIAL SHALL BE DISPOSED OF IN UPLAND (NON-WETLAND) AREAS AND ABOVE ORDINARY HIGH WATER OF ANY ADJACENT WATERCOURSE. THIS DOES NOT ELIMINATE THE NEED TO OBTAIN ANY OTHER LICENSES OR PERMITS THAT MAY BE REQUIRED BY ANY OTHER FEDERAL, STATE OR LOCAL AGENCY.

GUARDRAIL

(1) THE CONTRACTOR SHALL NOT REMOVE ANY SECTIONS OF EXISTING GUARDRAIL TO REWORK SHOULDERS OR FLATTEN SLOPES UNTIL THE ENGINEER CONCURS IN THE NECESSITY OF REMOVAL DUE TO CONSTRUCTION REQUIREMENTS AND THE APPROPRIATE WARNING DEVICES ARE INSTALLED. THE PROPOSED GUARDRAIL, INCLUDING ANY ANCHOR SYSTEM, SHALL BE INSTALLED QUICKLY TO MINIMIZE TRAFFIC EXPOSURE TO ANY HAZARD. NO PAYMENT WILL BE MADE FOR A SECTION OF PROPOSED GUARDRAIL, INCLUDING ANCHORS, UNTIL IT IS COMPLETE IN PLACE.

(3) IF ANY APPROACH END OF A SECTION OF GUARDRAIL OR BRIDGE RAIL MUST TEMPORARILY BE LEFT INCOMPLETE AND EXPOSED TO TRAFFIC, THE CONTRACTOR SHALL USE TWO (2) TEMPORARY BARRICADES OR DRUMS WITH TYPE “A” LIGHTS AND ROUNDED END ELEMENTS AS MINIMUM MEASURES TO PROTECT TRAFFIC FROM THE HAZARD OF AN EXPOSED END. ALL COST OF FURNISHING AND INSTALLING TEMPORARY BARRICADES OR DRUMS WITH TYPE “A” LIGHTS TO DELINEATE GUARDRAIL END AND A TEMPORARY ROUNDED END ELEMENT SHALL BE INCLUDED IN THE COST OF THE PROPOSED GUARDRAIL END TERMINAL.

MISCELLANEOUS

(2) THE CONTRACTOR SHALL BE REQUIRED TO REMOVE AND RESET MAILBOXES AND POSTS WHERE AND AS DIRECTED BY THE ENGINEER. COST TO BE INCLUDED IN PRICE BID FOR OTHER CONSTRUCTION ITEMS.

(3) NOTHING IN THE GENERAL NOTES OR SPECIAL PROVISIONS SHALL RELIEVE THE CONTRACTOR FROM HIS RESPONSIBILITIES TOWARD THE SAFETY AND CONVENIENCE OF THE GENERAL PUBLIC AND THE RESIDENTS ALONG THE PROPOSED CONSTRUCTION AREA.

PAVEMENT MARKINGS

TEMPORARY PAVEMENT MARKINGS ON INTERMEDIATE LAYERS

(2) TEMPORARY PAVEMENT LINE MARKINGS ON INTERMEDIATE LAYERS OF PAVEMENT SHALL BE REFLECTIVE TAPE OR REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT, UNMARKED SECTIONS SHALL NOT BE ALLOWED. THESE MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-05.20, PAINTED PAVEMENT MARKING (6” LINE), L.M.

FINAL PAVEMENT MARKING

(6) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

a. BROOMING & DE-GRASSING SHOULDERS SHALL INCLUDE CLIPPING OF MATERIAL INTERFERING WITH PROPER DRAINAGE OF ROADWAY (INCLUDING PAVED AND GRAVEL SHOULDERS), AS DIRECTED BY THE TDOT PROJECT ENGINEER.

b. ALL MATERIAL FROM CLIPPING, BROOMING AND DE-GRASSING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

c. ALL COSTS ASSOCIATED WITH PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

d. REMOVE ALL GARBAGE AND CONSTRUCTION DEBRIS FROM PROJECT. THE COST FOR THIS WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

(8) PERMANENT PAVEMENT LINE MARKINGS SHALL BE 6” ENHANCED FLATLINE THERMOPLASTIC INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK. SHORT UNMARKED SECTIONS SHALL NOT BE ALLOWED. PAVEMENT MARKINGS WILL BE MEASURED AND PAID FOR UNDER ITEM NO. 716-12.02, ENHANCED FLATLINE THERMO PVMT MRKNG (6IN LINE), L.M. THE CONTRACTOR SHALL HAVE THE OPTION OF USING REFLECTORIZED PAINT INSTALLED TO PERMANENT STANDARDS AT THE END OF EACH DAY'S WORK AND THEN INSTALLING THE PERMANENT MARKINGS AFTER THE PAVING OPERATION IS COMPLETED. THE TEMPORARY MARKINGS FOR THE FINAL SURFACE WILL NOT BE MEASURED AND PAID FOR DIRECTLY, BUT THE COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR THE PERMANENT MARKINGS.

SNOWPLOWABLE REFLECTIVE PAVEMENT MARKERS

(19) REMOVE EXISTING SNOWPLOWABLE MARKERS PRIOR TO PAVING AND/OR COLD PLANING. REMOVE ALL ADHESIVES PRIOR TO PAVING. PATCH ANY HOLES OR DIVOTS RESULTING FROM THE REMOVAL OF A MARKER IN A MANNER WHICH ENSURES A UNIFORM PAVED SURFACE. PATCH WORK SHALL BE INCLUDED WITH COST OF OTHER ITEMS OF CONSTRUCTION.

PAVEMENT

PAVING

(1) THE CONTRACTOR SHALL BE REQUIRED TO PAVE IN THE DIRECTION OF TRAFFIC.

RESURFACING

(4) WHERE DIRECTED BY THE TDOT ENGINEER, THE CONTRACTOR SHALL BE REQUIRED TO SHAPE PUBLIC SIDE ROADS, BUSINESS ENTRANCES, AND PRIVATE DRIVES, AS WELL AS CLEANING OF EXISTING DRAINS BEFORE PLACING MATERIALS. ALL COSTS ARE TO BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

(5) ALL PUBLIC SIDE ROADS SHALL BE PAVED ONE PAVER WIDTH THROUGH THE INTERSECTION AS A MINIMUM. A SATISFACTORY TRANSITION FROM THE NEW PAVEMENT TO THE EXISTING GRADE OF THE INTERSECTING PUBLIC ROAD OR BUSINESS ENTRANCE SHALL BE PROVIDED. SHOULD THE PAVEMENT OF THE INTERSECTING PUBLIC ROAD BE DISTRESSED, THE RESURFACING WIDTH MAY BE INCREASED TO THE NORMAL RIGHT OF WAY LINE.

(6) PRIVATE DRIVEWAYS, FIELD ENTRANCES, AND BUSINESS ENTRANCES WILL BE RESURFACED A PAVER WIDTH (LANE WIDTH) AS A MINIMUM. A PAVEMENT TAPER TO TRANSITION THE NEW PAVEMENT SHALL BE REQUIRED, IT SHALL BE BASED ON AN ADDITIONAL ONE FOOT OF WIDTH PER ONE INCH DEPTH OF PAVEMENT. IF THE SHOULDER IS NARROW ENOUGH THAT THE SUM OF THE SHOULDER AND THE TRANSITION ARE LESS THAN A PAVER WIDTH, THE TRANSITION SHALL OCCUR WITHIN THE PAVER WIDTH. IF THE SUM OF THE SHOULDER AND THE TRANSITION IS GREATER THAN A PAVER WIDTH (LANE WIDTH), THE TRANSITION SHALL OCCUR OUTSIDE OF THE PAVER WIDTH.

(9) IN ALL CASES, THE LENGTH OF THE PAVEMENT TRANSITION, THE THICKNESS AND WIDTH OF THE RESURFACING AND ANY ADDITIONAL PAVEMENT MATERIALS SHALL BE AS DIRECTED BY THE TDOT ENGINEER.

SIGNING

(12) ALL SIGNS WHICH INTERFERE WITH CONSTRUCTION WILL BE RELOCATED OUTSIDE LIMITS OF CONSTRUCTION BY THE CONTRACTOR. UPON COMPLETION OF CONSTRUCTION, THE CONTRACTOR WILL RESTORE THE SIGNS TO ORIGINAL LOCATION. THE CONTRACTOR SHALL CHECK WITH THE REGIONAL TRAFFIC ENGINEER PRIOR TO MOVING ANY PERMANENT SIGNS.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

(1) ADVANCED WARNING SIGNS SHALL NOT BE DISPLAYED MORE THAN FORTY-EIGHT (48) HOURS BEFORE PHYSICAL CONSTRUCTION BEGINS. SIGNS MAY BE ERECTED UP TO ONE WEEK BEFORE NEEDED, IF THE SIGN FACE IS FULLY COVERED.

(2) IF THE CONTRACTOR MOVES OFF THE PROJECT, HE SHALL COVER OR REMOVE ALL UNNEEDED SIGNS AS DIRECTED BY THE ENGINEER. COSTS OF REMOVAL, COVERING, AND REINSTALLING SIGNS SHALL NOT BE MEASURED AND PAID FOR SEPARATELY, BUT ALL COSTS SHALL BE INCLUDED IN THE ORIGINAL UNIT PRICE BID FOR ITEM NO. 712-06, SIGNS (CONSTRUCTION) PER SQUARE FOOT.

(3) A LONG TERM BUT SPORADIC USE WARNING SIGN, SUCH AS A FLAGGER SIGN, MAY REMAIN IN PLACE WHEN NOT REQUIRED PROVIDED THE SIGN FACE IS FULLY COVERED.

(4) TRAFFIC CONTROL DEVICES SHALL NOT BE DISPLAYED OR ERECTED UNLESS RELATED CONDITIONS ARE PRESENT NECESSITATING WARNING.

(5) USE OF BARRICADES, PORTABLE BARRIER RAILS, AND DRUMS SHALL BE LIMITED TO THE IMMEDIATE AREAS OF CONSTRUCTION WHERE A HAZARD IS PRESENT. THESE DEVICES SHALL NOT BE STORED ALONG THE ROADWAY WITHIN THIRTY (30) FEET OF THE EDGE OF THE TRAVELED WAY BEFORE OR AFTER USE UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL INCREASE TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. THESE DEVICES SHALL BE REMOVED FROM THE CONSTRUCTION WORK ZONE WHEN THE ENGINEER DETERMINES THEY ARE NO LONGER NEEDED. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

(6) THE CONTRACTOR SHALL NOT BE PERMITTED TO PARK ANY VEHICLES OR CONSTRUCTION EQUIPMENT DURING PERIODS OF INACTIVITY, WITHIN THIRTY (30) FEET OF THE EDGE OF PAVEMENT WHEN THE LANE IS OPEN TO TRAFFIC UNLESS PROTECTED BY GUARDRAIL, BRIDGE RAIL, AND/OR BARRIERS INSTALLED FOR OTHER PURPOSES FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. PRIVATELY OWNED VEHICLES SHALL NOT BE ALLOWED TO PARK WITHIN THIRTY (30) FEET OF AN OPEN TRAFFIC LANE AT ANY TIME UNLESS PROTECTED AS DESCRIBED ABOVE FOR ROADWAYS WITH CURRENT ADT'S LESS THAN 1500 AND DESIGN SPEED OF LESS THAN 60 MPH. THIS DISTANCE SHALL BE INCREASED TO FORTY-FIVE (45) FEET FOR ROADWAYS WITH CURRENT ADT'S OF 1500 OR GREATER AND DESIGN SPEED OF 60 MPH OR GREATER OR ON THE OUTSIDE OF A HORIZONTAL CURVE. WHERE THERE IS INSUFFICIENT RIGHT-OF-WAY TO PROVIDE FOR THIS REQUIRED SETBACK, THE CONTRACTOR SHALL DETERMINE THE ALTERNATE LOCATIONS AND REQUEST THE ENGINEER'S APPROVAL TO USE THEM.

(7) ALL DETOUR AND CONSTRUCTION SIGNING SHALL BE IN STRICT ACCORDANCE WITH THE MANUAL ON UNIFORM TRAFFIC CONTROL DEVICES.

(9) THE CONTRACTOR SHALL BE RESPONSIBLE FOR STAKING CONSTRUCTION SIGNS. THE COST OF THIS WORK SHALL BE INCLUDED IN ITEM NO. 712-06, SIGNS (CONSTRUCTION), S.F.

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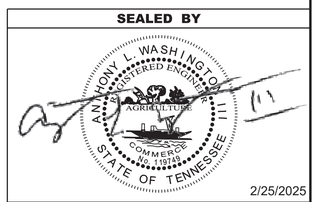

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GENERAL NOTES

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	STP/HSIP-57(93)	2D

SPECIAL NOTES

PAVEMENT

RESURFACING

(1) SURFACE IS TO BE CROWNED AS DIRECTED BY THE ENGINEER.

(2) THE CONTRACTOR WILL BE REQUIRED TO PERFORM THE FOLLOWING WORK:

A. BROOMING & DEGRASSING SHALL INCLUDE NOTCHING THE GRAVEL SHOULDER PRIOR TO MILLING. THE NOTCH SHALL BE 1.5 IN DEEP AND 2 FT WIDE, OR AS DIRECTED BY THE TDOT PROJECT ENGINEER.

B. ALL MATERIAL FROM NOTCHING AND BROOMING SHOULDERS SHALL BE PICKED UP, REMOVED AND PROPERLY DISPOSED AS DIRECTED BY THE TDOT PROJECT ENGINEER.

C. ALL COSTS ASSOCIATED WITH NOTCHING, PICKING UP, REMOVAL AND PROPER DISPOSAL SHALL BE PAID FOR UNDER ITEM NO. 208-01.05.

(4) THE BITUMINOUS MATERIAL DESIGNATED TO RESTORE THE COLD PLANING AREA WILL BE PLACED WITHIN 96 HOURS OF THE COMMENCEMENT OF COLD PLANING OPERATIONS. COLD PLANING OPERATIONS WILL BE LIMITED TO AN AREA EQUAL TO THAT WHICH CAN BE COVERED WITH BITUMINOUS MATERIAL WITHIN THE TIME LIMITS SPECIFIED, EVEN IF COLD PLANING OPERATIONS MUST BE SUSPENDED UNTIL PAVING CATCHES UP.

(5) FEATHER SURFACE MIX TO ENDS OF BRIDGES THAT ARE NOT TO BE PAVED.

PAVEMENT MARKING

(1) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PAINTED MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-05.20

(2) UNDER THE DIRECTION OF THE ENGINEER, THE CONTRACTOR MAY BE REQUIRED TO APPLY PLASTIC MARKINGS IN THE PAVEMENT AREAS NOT SPECIFICALLY DETAILED IN THE PLANS. PAYMENT WILL BE MADE AT THE UNIT PRICE BID FOR ITEM NO. 716-12.02.

SIGNS

(1) IF THE CONTRACTOR ELECTS TO UTILIZE SIGN POST ANCHORS (STUBS) FOR SIGN ERECTION, THESE SHALL BE REMOVED WHEN THE SIGNS ARE REMOVED TO AVOID FUTURE DAMAGE TO MOWERS OR OTHER MACHINERY.

MISCELLANEOUS

(1) ITEM 303-02 TO BE PLACED BEFORE PLACING SURFACING MATERIAL.

(2) THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE PROTECTION OF ANY SIGNS AND MAILBOXES DURING THE OPERATION. ANY SIGNS OR MAILBOXES DAMAGED AS A RESULT OF THE OPERATIONS SHALL BE REPLACED AT THE CONTRACTOR'S EXPENSE.

CONSTRUCTION WORK ZONE & TRAFFIC CONTROL

(1) THE CONTRACTOR SHALL KEEP TWO TRAFFIC LANES, ONE IN EACH DIRECTION, OPEN TO TRAFFIC DURING NON-WORK HOURS OR NON-WORK DAYS.

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SPECIAL NOTES

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	STP/HSIP-57(93)	2E

ENVIRONMENTAL NOTES

ENVIRONMENTAL GENERAL NOTES

NATURAL RESOURCES

(4) THE OPERATION OF EQUIPMENT IN WATERS OF THE STATE/U.S., INCLUDING WETLANDS AND EPHEMERAL, INTERMITTENT, AND PERENNIAL STREAMS, IS NOT ALLOWED.

(9) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS PRIOR TO ANY CONSTRUCTION AND MAINTENANCE ACTIVITIES TO ENSURE THAT ENVIRONMENTAL FEATURES (E.G., STREAMS, WETLANDS, SPRINGS, ETC.) ARE NOT IMPACTED BEYOND PERMITTED LOCATIONS. IF THE CONTRACTOR OR TDOT INSPECTOR IS UNSURE OF THE IDENTITY OF AN ENVIRONMENTAL FEATURE, THE INSPECTOR SHALL CONTACT THE TDOT REGION ENVIRONMENTAL TECH GROUP IMMEDIATELY.

SPECIES

(11) SHOULD CLIFF SWALLOW OR BARN SWALLOW NESTS, EGGS, OR BIRDS (YOUNG AND ADULTS) BE PRESENT, THE CONTRACTOR SHALL CONTACT THE REGIONAL ECOLOGY OFFICE TO DETERMINE IF SEASONAL RESTRICTIONS WILL BE NECESSARY. GENERALLY, BIRDS, NESTS, AND EGGS MAY NOT BE DISTURBED BETWEEN APRIL 15 AND JULY 31. FROM AUGUST 1 TO APRIL 14, NESTS CAN BE REMOVED OR DESTROYED SO LONG AS BIRDS OR EGGS ARE NOT PRESENT, AND MEASURES IMPLEMENTED TO PREVENT FUTURE NEST BUILDING AT THE SITE (I.E., CLOSING OFF AREA USING NETTING).

(12) IF THE REMOVAL OF ANY TREES WITH A DIAMETER AT BREAST HEIGHT (DBH) GREATER THAN 3 INCHES IS DEEMED NECESSARY THE TDOT SUPERVISOR SHALL CONTACT THE TDOT ENVIRONMENTAL DIVISION, ECOLOGY SECTION IMMEDIATELY.

PERMITS, PLANS & RECORDS

(15) IF A CHANGE IN PROJECT SCOPE OCCURS DURING CONSTRUCTION, INCLUDING VALUE ENGINEERING, THE TDOT PERMIT SECTION SHALL BE CONTACTED TO DETERMINE WHETHER PERMIT REVISIONS ARE NEEDED. THE ENGINEERING PRODUCTION SUPPORT DIVISION SHALL BE CONTACTED TO DETERMINE IF ANY PLAN REVISIONS ARE NEEDED.

ENVIRONMENTAL SPECIAL NOTES

ENVIRONMENTAL

(1) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION COMPLIANCE AND FIELD SERVICES OFFICE SHALL BE INVITED TO ALL PRE-CONSTRUCTION MEETINGS.

ECOLOGY

(2) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ADVISE THE CONTRACTOR DURING THE PRE-CONSTRUCTION MEETING WHEN ENVIRONMENTAL DIVISION PERSONNEL OR A DESIGNATED CONSULTANT WILL NEED TO BE ONSITE FOR WORK BEING DONE WHICH COULD AFFECT WATERS OF THE STATE/U.S. OR SPECIES.

(3) STAFF FROM THE TDOT ENVIRONMENTAL DIVISION OR A DESIGNEE SHALL ATTEND THE PRE-CONSTRUCTION MEETING FOR ALL PROJECTS WHICH HAVE THREATENED OR ENDANGERED SPECIES OR CRITICAL HABITAT PROXIMAL TO SCHEDULED WORK. THIS WILL PROVIDE THE OPPORTUNITY TO ENSURE THAT PERSONNEL INCLUDING THE CONTRACTOR'S PERSONNEL AND SUBCONTRACTORS ARE MADE AWARE OF THE NECESSARY PRECAUTIONS THAT MUST BE FOLLOWED.

(4) ALL PROJECTS WITH LEGALLY PROTECTED SPECIES OR CRITICAL HABITAT IDENTIFIED SHALL HAVE MEASURES IN PLACE TO CONTAIN CONCRETE DUST, CEMENT DUST AND ALL OTHER MATERIALS. THESE MATERIALS ARE NOT ALLOWED TO ENTER WATERS OF THE STATE/U.S.

SCOPE OF WORK

(6) THIS PROJECT INCLUDES SCRUB SEALING, TLD, EXISTING PAVEMENT REPAIR, PAVEMENT MARKINGS, GUARDRAIL, TEMPORARY TRAFFIC CONTROL, AND BROOMING AND DEGRASSING SHOULDERS.

EROSION PREVENTION AND SEDIMENT CONTROL GENERAL NOTES

DISTURBED AREA

(1) IF DISTURBED ACREAGE IS EQUAL TO ONE ACRE OR MORE, PLEASE CONTACT TDOT ENVIRONMENTAL DIVISION, PERMITS SECTION AS SOON AS POSSIBLE BECAUSE AN NPDES PERMIT WILL BE REQUIRED.

SEDIMENT CONTROL

(6) EPSC MEASURES SHALL BE INSTALLED AND FUNCTIONAL PRIOR TO ANY EARTH MOVING OPERATIONS AND SHALL BE MAINTAINED THROUGHOUT THE CONSTRUCTION PERIOD EXCEPT AS SUCH WORK MAY BE NECESSARY TO INSTALL EPSC MEASURES.

(8) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT THE OFFSITE MIGRATION OR DEPOSIT OF SEDIMENT OFF THE PROJECT LIMITS (E.G. R.O.W., EASEMENTS, ETC.), INTO WATERS OF THE STATE/U.S., OR ONTO ROADWAYS USED BY THE GENERAL PUBLIC. IF SEDIMENT ESCAPES THE CONSTRUCTION SITE, OFFSITE ACCUMULATIONS OF SEDIMENT THAT HAVE NOT REACHED A STREAM MUST BE REMOVED AT A FREQUENCY SUFFICIENT TO MINIMIZE OFFSITE IMPACTS (E.G., FUGITIVE SEDIMENT THAT HAS ESCAPED THE CONSTRUCTION SITE AND HAS COLLECTED IN A STREET MUST BE REMOVED SO THAT IT IS NOT SUBSEQUENTLY WASHED INTO STORM SEWERS AND STREAMS BY THE NEXT RAIN AND/OR SO THAT IT DOES NOT POSE A SAFETY HAZARD TO USERS OF PUBLIC STREETS). ARRANGEMENTS CONCERNING REMOVAL OF SEDIMENT ON ADJOINING PROPERTY MUST BE NEGOTIATED WITH THE ADJOINING PROPERTY OWNER BEFORE REMOVAL OF SEDIMENT.

GOOD HOUSEKEEPING MEASURES & WASTE DISPOSAL

(29) THE CONTRACTOR SHALL ESTABLISH AND MAINTAIN A PROACTIVE METHOD TO PREVENT LITTER AND CONSTRUCTION WASTES FROM ENTERING WATERS OF THE STATE/U.S. THESE MATERIALS SHALL BE REMOVED FROM STORMWATER EXPOSURE PRIOR TO ANTICIPATED STORM EVENTS OR BEFORE BEING CARRIED OFFSITE BY WIND, OR OTHERWISE PREVENTED FROM BECOMING A POLLUTANT SOURCE FOR STORMWATER DISCHARGES. AFTER USE, MATERIALS USED FOR EPSC SHALL BE REMOVED FROM THE SITE.

(30) THE CONTRACTOR SHALL TAKE APPROPRIATE STEPS TO ENSURE THAT PETROLEUM PRODUCTS OR OTHER CHEMICAL POLLUTANTS ARE PREVENTED FROM ENTERING WATERS OF THE STATE/U.S. ALL EQUIPMENT REFUELING, SERVICING, AND STAGING AREAS SHALL COMPLY WITH ALL LOCAL, STATE, AND FEDERAL LAWS, RULES, REGULATIONS, AND ORDINANCES, INCLUDING THOSE OF THE NATIONAL FIRE PROTECTION ASSOCIATION. APPROPRIATE CONTAINMENT MEASURES FOR THESE AREAS SHALL BE USED.

(31) CONTRACTORS SHALL PROVIDE DESIGNATED TRUCK WASHOUT AREAS ON THE SITE. THESE AREAS MUST BE SELF CONTAINED, NOT CONNECTED TO ANY STORMWATER OUTLET OF THE SITE, AND PROPERLY SIGNED. WASH DOWN OR WASTE DISCHARGE OF CONCRETE TRUCKS SHALL NOT BE PERMITTED ONSITE UNLESS PROPER SETTLEMENT AREAS HAVE BEEN PROVIDED IN ACCORDANCE WITH BOTH STATE AND FEDERAL REGULATIONS.

(32) WHEEL WASH WATER SHALL BE COLLECTED AND ALLOWED TO SETTLE OUT SUSPENDED SOLIDS PRIOR TO DISCHARGE. WHEEL WASH WATER SHALL NOT BE DISCHARGED DIRECTLY INTO ANY STORMWATER SYSTEM OR STORMWATER TREATMENT SYSTEM.

(33) IF PORTABLE SANITARY FACILITIES ARE PROVIDED ON CONSTRUCTION SITES, SANITARY WASTE SHALL BE COLLECTED FROM THE PORTABLE UNITS IN A TIMELY MANNER BY A LICENSED WASTE MANAGEMENT CONTRACTOR OR AS REQUIRED BY ANY REGULATIONS. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF SANITARY WASTE.

(34) ONLY CONSTRUCTION PRODUCTS NEEDED SHALL BE STORED ONSITE BY THE CONTRACTOR. THE CONTRACTOR SHALL STORE ALL MATERIALS UNDER COVER AND IN APPROPRIATE CONTAINERS. PRODUCTS MUST BE STORED IN ORIGINAL CONTAINERS AND LABELED. MATERIAL MIXING SHALL BE CONDUCTED IN ACCORDANCE WITH THE MANUFACTURER'S RECOMMENDATIONS. THE CONTRACTOR'S RESPONSIBLE PARTY SHALL INSPECT MATERIALS STORAGE AREAS REGULARLY TO ENSURE PROPER USE AND DISPOSAL.

(35) WHEN POSSIBLE, ALL PRODUCTS SHALL BE USED COMPLETELY BEFORE PROPERLY DISPOSING OF THE CONTAINER OFFSITE. THE MANUFACTURER'S DIRECTIONS FOR DISPOSAL OF MATERIALS AND CONTAINERS SHALL BE FOLLOWED.

(36) ALL PAINT CONTAINERS SHALL BE TIGHTLY SEALED AND STORED WHEN NOT REQUIRED FOR USE. EXCESS PAINT SHALL BE DISPOSED OF ACCORDING TO THE MANUFACTURER'S INSTRUCTIONS AND APPLICABLE STATE AND LOCAL REGULATIONS.

(37) ALL HAZARDOUS WASTE MATERIALS SHALL BE DISPOSED OF IN A MANNER WHICH IS COMPLIANT WITH LOCAL OR STATE REGULATIONS. SITE PERSONNEL SHALL BE INSTRUCTED IN THESE PRACTICES, AND THE INDIVIDUAL DESIGNATED AS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR SEEING THAT THESE PRACTICES ARE FOLLOWED. THE CONTRACTOR SHALL OBTAIN ANY AND ALL NECESSARY PERMITS TO DISPOSE OF HAZARDOUS MATERIAL.

(38) OPEN BURNING IS PROHIBITED UNLESS IT IS SPECIFICALLY ALLOWED BY LAW. IF ALLOWED, NATURAL VEGETATION, TREES, AND UNTREATED LUMBER SHALL BE THE ONLY MATERIALS THAT CAN BE OPEN BURNED. THE CONTRACTOR SHALL BE RESPONSIBLE FOR OBTAINING ALL APPLICABLE STATE AND LOCAL PERMITS PRIOR TO ANY BURNING.

(39) DISPOSAL OF ONSITE VEGETATION AND TREES BY CHIPPING THEM INTO MULCH IS PREFERABLE TO OPEN BURNING. THIS MULCH MAY BE USED AS AN ONSITE SOIL STABILIZATION MEASURE WHERE APPROPRIATE.

(40) WASTE MATERIAL (EARTH, ROCK, ASPHALT, CONCRETE, ETC.) NOT REQUIRED FOR THE CONSTRUCTION OF THE PROJECT WILL BE DISPOSED OF BY THE CONTRACTOR. IMPACTS TO WATERS OF THE STATE/U.S. SHALL BE AVOIDED IF POSSIBLE. IF UNAVOIDABLE, THE CONTRACTOR WILL OBTAIN ANY AND ALL NECESSARY PERMITS INCLUDING, BUT NOT LIMITED TO NPDES, AQUATIC RESOURCES ALTERATION PERMIT(S), CORPS OF ENGINEERS SECTION 404 PERMITS, AND TVA SECTION 26A PERMITS TO DISPOSE OF WASTE MATERIALS.

SPILL PREVENTION, MANAGEMENT & NOTIFICATION

(44) ALL ONSITE VEHICLES SHALL BE MONITORED FOR LEAKS AND RECEIVE REGULAR PREVENTIVE MAINTENANCE TO REDUCE THE CHANCE OF LEAKAGE AND SPILLS.

(45) FOR ALL HAZARDOUS MATERIALS STORED ONSITE, THE MANUFACTURER'S RECOMMENDED METHODS FOR SPILL CLEAN UP SHALL BE CLEARLY POSTED. SITE PERSONNEL SHALL BE MADE AWARE OF THE PROCEDURES AND THE LOCATIONS OF THE INFORMATION AND CLEANUP SUPPLIES.

(46) APPROPRIATE CLEANUP MATERIALS AND EQUIPMENT SHALL BE MAINTAINED BY THE CONTRACTOR IN THE MATERIALS STORAGE AREA ONSITE AND UNDER COVER. SPILL RESPONSE EQUIPMENT SHALL BE INSPECTED AND MAINTAINED BY THE CONTRACTOR AS NECESSARY TO REPLACE ANY MATERIALS USED IN SPILL RESPONSE ACTIVITIES.

(47) ALL SPILLS SHALL BE CLEANED IMMEDIATELY AFTER DISCOVERY AND THE MATERIALS DISPOSED OF PROPERLY. THE SPILL AREA SHALL BE KEPT WELL VENTILATED AND PERSONNEL WILL WEAR APPROPRIATE PROTECTIVE CLOTHING TO PREVENT INJURY FROM CONTACT WITH A HAZARDOUS SUBSTANCE.

(48) THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE THE SPILL PREVENTION AND CLEANUP COORDINATOR. THE CONTRACTOR IS RESPONSIBLE FOR ENSURING THAT THE SITE SUPERINTENDENT HAS HAD APPROPRIATE TRAINING FOR HAZARDOUS MATERIALS HANDLING, SPILL MANAGEMENT, AND CLEANUP.

(49) IF AN OIL SHEEN IS OBSERVED ON SURFACE WATER (E.G. SETTLING PONDS, DETENTION PONDS, SWALES), ACTION SHALL BE TAKEN IMMEDIATELY TO REMOVE THE MATERIAL CAUSING THE SHEEN. THE CONTRACTOR SHALL USE APPROPRIATE MATERIALS TO CONTAIN AND ABSORB THE SPILL. THE SOURCE OF THE OIL SHEEN WILL ALSO BE IDENTIFIED AND REMOVED OR REPAIRED AS NECESSARY TO PREVENT FURTHER RELEASES.

(50) FERTILIZERS SHALL BE APPLIED ONLY IN THE AMOUNTS SPECIFIED. ONCE APPLIED, FERTILIZERS SHALL BE WORKED INTO THE SOIL TO LIMIT THE EXPOSURE TO STORMWATER.

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STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL NOTES

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	STP/HSIP-57(93)	2E1

ENVIRONMENTAL NOTES (CONT.)

(51) IF A SPILL OCCURS THE CONTRACTOR'S RESPONSIBLE PARTY SHALL BE RESPONSIBLE FOR COMPLETING THE SPILL REPORTING FORM AND FOR REPORTING THE SPILL TO THE TDOT PROJECT RESPONSIBLE PARTY. ALL SPILLS MUST BE REPORTED TO THE APPROPRIATE AGENCY, AND MEASURES SHALL BE TAKEN IMMEDIATELY TO PREVENT THE POLLUTION OF WATERS OF THE STATE/U.S., INCLUDING GROUNDWATER, SHOULD A SPILL OCCUR.

(52) WHERE A RELEASE CONTAINING A HAZARDOUS SUBSTANCE IN AN AMOUNT EQUAL TO OR IN EXCESS OF A REPORTABLE QUANTITY ESTABLISHED UNDER EITHER 40 CFR 117 OR 40 CFR 302 OCCURS DURING A 24 HOUR PERIOD, SEE THE LATEST TENNESSEE GENERAL PERMIT NO. TNR100000 STORMWATER DISCHARGES FROM CONSTRUCTION ACTIVITIES SECTION 5.1 FOR REPORTING REQUIREMENTS.

(53) CONTRACTOR'S BULK FUEL AND PETROLEUM PRODUCTS STORED ONSITE OR ADJACENT TO THE R.O.W. IN ABOVE GROUND STORAGE CONTAINERS WITH A COMBINED CAPACITY OF 1320 GALLONS OR MORE SHALL HAVE SECONDARY CONTAINMENT. THE CONTRACTOR SHALL BE RESPONSIBLE FOR PREPARING A SPILL PREVENTION CONTROL AND COUNTERMEASURE (SPCC) PLAN FOR THE BULK STORAGE AND BE SOLELY RESPONSIBLE FOR OBTAINING ANY NECESSARY LOCAL, STATE, AND FEDERAL PERMITS. THE SPCC PLAN AND/OR PERMITS SHALL BE KEPT ONSITE AND A COPY PROVIDED TO THE TDOT PROJECT RESPONSIBLE PARTY PRIOR TO STORING 1320 GALLONS ON SITE.

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DEPARTMENT OF TRANSPORTATION

ENVIRONMENTAL NOTES

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TRAFFIC CONTROL SIGN TABULATION (RESURFACING)

M.U.T.C.D. SIGN NO.	LEGEND \ DESCRIPTION	SIZE IN INCHES L x W	S.F.	TOTAL NUMBER REQUIRED	ITEM NO. 712-06 S.F.
G20-1	ROAD WORK NEXT 15 MILES	48" x 24"	8	2	16
G20-2	END ROAD WORK	48" x 24"	8	48	384
W20-1	ROAD WORK 1 MILE	48" x 48"	16	2	32
W20-1	ROAD WORK 1/2 MILE	48" x 48"	16	2	32
W20-1	ROAD WORK 1000 FT	48" x 48"	16	2	32
W20-1	ROAD WORK AHEAD	48" x 48"	16	46	736
W20-4	ONE LANE ROAD 1000 FT - PORTABLE	48" x 48"	16	2	32
W20-7A	FLAGGER SYMBOL - PORTABLE	48" x 48"	16	2	32
W21-2	FRESH OIL - PORTABLE	48" x 48"	16	2	32
W21-5	SHOULDER WORK	48" x 48"	16	4	64
				TOTAL	1392

PROPOSED GUARDRAIL (RESURFACING)

SIDE LT	SIDE RT	LOG MILE	GUARDRAIL: GUARDRAIL BEAM TRANSITION 27 IN TO 31 IN 705-02.10 (EACH)	GUARDRAIL: EARTH PAD FOR TYPE 38 GR END TREATMENT 705-04.09 (EACH)	GUARDRAIL: GUARDRAIL REMOVED 706-01 (L.F.)	GUARDRAIL: RADIUS RAIL 706-06.03 (L.F.)	TERMINAL: TYPE 13 MASH TL3 (9.375') 705-06.10 (EACH)	TERMINAL: IN-LINE MASH TL3 705-06.11 (EACH)	TERMINAL: TYPE 38 MASH TL3 (46.875') 705-06.20 (EACH)	TERMINAL: TYPE 21 MASH TL2 (21.875') 705-06.30 (EACH)
	X	5.802	1	1	50				1	
X		5.808	1	1	50				1	
	X	5.838	1	1	50					1
X		5.841	1	1	50					1
	X	9.286	1	1	50				1	
X		9.297	1	1	50				1	
X		9.332	1	1	25	20	1	1		
X		9.341	1	1	25	20	1	1		
X		9.416	1	1	50				1	
	X	10.037	1	1	50				1	
X		10.041	1	1	50				1	
	X	10.100	1	1	50				1	
X		10.312	1	1	50				1	
	X	10.312	1	1	50				1	
X		10.375	1	1	50				1	
TOTALS			15	15	700	40	2	2	11	2

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	STP/HSIP-57(93)	2F

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DEPARTMENT OF TRANSPORTATION

TABULATED QUANTITIES

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	STP/HSIP-57(93)	3

UTILITY NOTES

UTILITY

(2) UNLESS OTHERWISE NOTED, ALL UTILITY ADJUSTMENTS WILL BE PERFORMED BY THE UTILITY OR ITS REPRESENTATIVE. THE CONTRACTOR AND UTILITY OWNERS WILL BE REQUIRED TO COOPERATE WITH EACH OTHER IN ORDER TO EXPEDITE THE WORK REQUIRED BY THIS CONTRACT. ON CONTRACTS WHERE CONSTRUCTION STAKES, LINES, AND GRADES ARE CONTRACT ITEMS, THE CONTRACTOR WILL BE REQUIRED TO PROVIDE RIGHT-OF-WAY OR SLOPE STAKES, DITCH OR STREAM BED GRADES, OR OTHER ESSENTIAL SURVEY STAKING TO PREVENT CONFLICTS WITH THE HIGHWAY CONSTRUCTION. FREQUENTLY, THIS WILL BE REQUIRED AS THE FIRST ITEM OF WORK AND AT ANY LOCATION ON THE PROJECT DIRECTED BY THE ENGINEER.

(3) THE CONTRACTOR WILL PROVIDE ALL NECESSARY PROTECTIVE MEASURES TO SAFEGUARD EXISTING UTILITIES FROM DAMAGE DURING CONSTRUCTION OF THIS PROJECT. IN THE EVENT THAT SPECIAL EQUIPMENT IS REQUIRED TO WORK OVER AND AROUND THE UTILITIES, THE CONTRACTOR WILL BE REQUIRED TO FURNISH SUCH EQUIPMENT. THE COST OF PROTECTING UTILITIES FROM DAMAGE AND FURNISHING SPECIAL EQUIPMENT WILL BE INCLUDED IN THE PRICE BID FOR OTHER ITEMS OF CONSTRUCTION.

(4) PRIOR TO SUBMITTING HIS BID, THE CONTRACTOR WILL BE SOLELY RESPONSIBLE FOR CONTACTING OWNERS OF ALL AFFECTED UTILITIES IN ORDER TO DETERMINE THE EXTENT TO WHICH UTILITY RELOCATIONS AND/OR ADJUSTMENTS WILL HAVE UPON THE SCHEDULE OF WORK FOR THE PROJECT. WHILE SOME WORK MAY BE REQUIRED 'AROUND' UTILITY FACILITIES THAT WILL REMAIN IN PLACE, OTHER UTILITY FACILITIES MAY NEED TO BE ADJUSTED CONCURRENTLY WITH THE CONTRACTOR'S OPERATIONS. ADVANCE CLEAR CUTTING MAY BE REQUIRED BY THE ENGINEER AT ANY LOCATION WHERE CLEARING IS CALLED FOR IN THE SPECIFICATIONS AND CLEAR CUTTING IS NECESSARY FOR A UTILITY RELOCATION. ANY ADDITIONAL COST WILL BE INCLUDED IN THE UNIT PRICE BID FOR THE CLEARING ITEM SPECIFIED IN THE PLANS.

(5) THE CONTRACTOR SHALL NOTIFY EACH INDIVIDUAL UTILITY OWNER OF HIS PLAN OF OPERATION IN THE AREA OF THE UTILITIES. PRIOR TO COMMENCING WORK, THE CONTRACTOR SHALL CONTACT THE UTILITY OWNERS AND REQUEST THEM TO PROPERLY LOCATE THEIR RESPECTIVE UTILITY ON THE GROUND. THIS NOTIFICATION SHALL BE GIVEN AT LEAST THREE (3) BUSINESS DAYS PRIOR TO COMMENCEMENT OF OPERATIONS AROUND THE UTILITY IN ACCORDANCE WITH TCA 65-31-106. NOTIFICATION BY CALLING THE TENNESSEE ONE CALL SYSTEM, INC AT 1-800-351-1111 WILL BE REQUIRED.

UTILITY OWNERS

ELECTRIC:
CHICKASAW ELECTRIC CO.
17970 HWY 64 East
Somerville. Tn. 38068
CONTACT: Loyd Muncy
OFFICE PHONE: 901 466 2536
Email: lmuncy@chicksaw.coop

FIBER OPTIC:
CENTURYLINK (QWEST) / LUMEN (LEVEL 3)
8110 Cordova Rd. Ste. 101
Cordova, Tn. 38016
CONTACT: Brian McGREGOR
OFFICE PHONE: 901 435 2025
Email: brian.mcgregor@lumen.com

WATER & SEWER:
CITY OF MOSCOW
155 Fourth Ave
Moscow TN 38057
CONTACT: Ren Weatherall
OFFICE PHONE: 901 877 3585
Email: cityclerk2323@gmail.com

COMMUNICATIONS:
VERIZON / XO COMMUNICATIONS
5127 Truse Rd
Memphis, TN 38117
CONTACT: Robert Stafford
OFFICE PHONE: 901 239 2912
Email: robert.stafford@verizon.com

COMMUNICATIONS:
AT&T
315 E. College Street
Jackson, TN. 38301
CONTACT: Daniel R. Potts
OFFICE PHONE: 901 488 2359
Email: dp7607@att.com

GAS:
HARDEMAN/FAYETTE
15175 HWY 57 East / P.O. Box 7.
Moscow, Tn. 38057
CONTACT: Clay Joyner
OFFICE PHONE: 901 877 6236
Email: cjoyner@hfutilitydistrict.com

WATER & SEWER:
CITY OF ROSSVILLE
360 Morrison Ave
Rossville TN 38066
CONTACT: Joseph Howell
OFFICE PHONE: 901 853 4681
Email: j.dhowell@gmail.com

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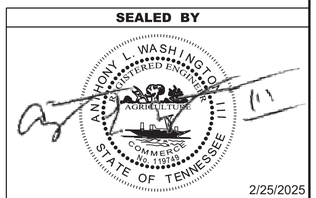

2/25/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

UTILITY NOTES
AND
UTILITY OWNERS

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TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF	2025	STP/HSIP-57(93)	4

PAVEMENT EDGE DROP-OFF TRAFFIC CONTROL NOTES

A. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES OR TRAFFIC LANE AND SHOULDER WHERE THE TRAFFIC LANE IS BEING USED BY TRAFFIC, CAUSED BY BASE, PAVING OR RESURFACING:

1. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 0.75 INCH AND NOT EXCEEDING 1.75 INCHES:

   a. WARNING SIGNS, UNEVEN LANES (W8-11) AND/OR SHOULDER DROP-OFF WITH PLAQUE (W8-17 AND W8-17P), SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

   b. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY ADDED PAVEMENT SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

   c. DIFFERENCES IN ELEVATION BETWEEN ADJACENT TRAFFIC LANES BEING UTILIZED BY TRAFFIC CAUSED BY COLD PLANING SHALL BE ELIMINATED WITHIN THREE WORKDAYS.

   d. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE TRAFFIC LANE BEING UTILIZED BY TRAFFIC AND SHOULDER THE DIFFERENCE IN ELEVATION SHALL BE ELIMINATED WITHIN SEVEN WORKDAYS AFTER THE CONDITION IS CREATED.

2. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 1.75 INCHES AND NOT EXCEEDING 6 INCHES, TRAFFIC IS NOT TO BE ALLOWED TO TRAVERSE THIS DIFFERENCE IN ELEVATION.

   a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

      (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

      (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

   b. IF THE DIFFERENCE IN ELEVATION IS ELIMINATED OR DECREASED TO 2 INCHES OR LESS BY THE END OF EACH WORKDAY, CONES MAY BE USED DURING DAYLIGHT HOURS IN LIEU OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES MENTIONED IN PARAGRAPH a, PROVIDED WARNING SIGNS ARE ERECTED. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

   c. WHEN THE DIFFERENCE IN ELEVATION IS BETWEEN THE THROUGH TRAFFIC LANE AND THE SHOULDER AND THE ELEVATION DIFFERENCE IS LESS THAN 3 INCHES, THE CONTRACTOR MAY USE WARNING SIGNS AND/OR PROTECTIVE DEVICES AS APPLICABLE AND APPROVED BY THE REGIONAL TRAFFIC ENGINEER. SEE PARAGRAPH a REGARDING USE OF DRUMS, BARRICADES OR OTHER APPROVED PROTECTIVE DEVICES. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) WILL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

   IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 2 MILES IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

3. DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 6 INCHES BUT NOT EXCEEDING 18 INCHES, THE CONTRACTOR, WITH THE ENGINEER'S APPROVAL, MAY UTILIZE ONE OF THE FOLLOWING:

   a. THE CONTRACTOR SHALL ACCOMPLISH SEPARATION BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

      (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

      (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

   IN ORDER TO USE THIS METHOD, THE CONTRACTOR MUST REDUCE THE DIFFERENCE IN ELEVATION TO 6 INCHES OR LESS BY THE END OF THE WORKDAY THAT THE CONDITION IS CREATED.

   b. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a, AND CONSTRUCT A STONE WEDGE WITH A 4:1 SLOPE, OR FLATTER, TO ELIMINATE THE VERTICAL OFFSET IF THE LOWER ELEVATION IS AT OR BELOW SUBGRADE AT THE END OF EACH DAY.

   c. THE CONTRACTOR SHALL PROVIDE DRUMS, BARRICADES OR OTHER APPROVED SEPARATION DEVICES AS SPECIFIED IN PARAGRAPH a AND IF THE LOWER ELEVATION IS BASE STONE OR ASPHALT PAVEMENT, PLACEMENT OF SUBSEQUENT LAYERS OF PAVEMENT MUST BEGIN THE NEXT WORK DAY AND PROGRESS CONTINUOUSLY UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED OR REDUCED TO SIX INCHES OR LESS.

   d. THE CONTRACTOR SHALL PROVIDE SEPARATION BY PORTABLE BARRIER RAIL.

   FOR PRECEDING CONDITIONS a, b, AND c, THE CONTRACTOR SHALL USE THE SHOULDER DROP-OFF WARNING SIGN WITH PLAQUE (W8-17 AND W8-17P). IT SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN THE SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. IN THESE SITUATIONS, THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

4. FOR DIFFERENCES IN ELEVATION BETWEEN ADJACENT ROADWAY ELEMENTS GREATER THAN 18 INCHES.

   SEPARATION WILL BE PROVIDED BY USE OF PORTABLE BARRIER RAIL.

   IN THIS SITUATION THE CONTRACTOR SHALL LIMIT HIS OPERATIONS TO ONE WORK ZONE NOT EXCEEDING 1 MILE IN LENGTH UNLESS OTHERWISE NOTED ON THE PLANS OR APPROVED BY THE ENGINEER. ONCE THE CONTRACTOR BEGINS WORK IN A WORK ZONE, A CONTINUOUS OPERATION SHALL BE MAINTAINED UNTIL THE DIFFERENCE IN ELEVATION IS ELIMINATED. SIMULTANEOUS WORK ON SEPARATE ROADWAYS OF DIVIDED HIGHWAYS WILL BE CONSIDERED INDEPENDENTLY IN REGARD TO RESTRICTION OF WORK ZONE ACTIVITY.

B. IF THE DIFFERENCE IN ELEVATION IS WITHIN 30 FEET OF THE NEAREST TRAFFIC LANE BEING USED BY TRAFFIC CAUSED BY GRADING, EXCAVATION FOR UTILITIES, DRAINAGE STRUCTURES, UNDERCUTTING, ETC.:

1. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 3/4 INCH AND NOT EXCEEDING 2 INCHES.

   a. WARNING SIGNS (UNEVEN LANES AND/OR SHOULDER DROP-OFF) SHALL BE PLACED IN ADVANCE OF AND THROUGHOUT THE EXPOSED AREA. MAXIMUM SPACING BETWEEN SIGNS SHALL BE 2,000 FEET WITH A MINIMUM OF 2 SIGNS PER EXPOSED AREA. WHERE UNEVEN PAVEMENT IS ENCOUNTERED, SIGNS SHALL BE PLACED ON EACH SIDE OF THE ROADWAY.

2. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 2 INCHES AND NOT EXCEEDING 6 INCHES:

   a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

      (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

      (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

3. IF THE DIFFERENCE IN ELEVATION IS WITHIN 8 FEET OF THE NEAREST TRAFFIC LANE WITH DIFFERENCE IN ELEVATION GREATER THAN 6 INCHES:

   a. SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

      (1) WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

      (2) WHERE POSTED SPEEDS ARE LESS THAN 50 MPH THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

   b. ELIMINATE VERTICAL OFFSET BY CONSTRUCTING A STONE WEDGE OR GRADING TO A 4:1 SLOPE, OR FLATTER, OR USE PORTABLE BARRIER RAIL.

   THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE WITHIN 8 FEET OF A TRAFFIC LANE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

C. IF THE DIFFERENCE IN ELEVATION IS FARTHER THAN 8 FEET FROM THE NEAREST TRAFFIC LANE BUT NOT MORE THAN 30 FEET FROM THE NEAREST TRAFFIC LANE:

SEPARATION SHALL BE ACCOMPLISHED BY DRUMS, BARRICADES OR OTHER APPROVED DEVICES IN ACCORDANCE WITH THE FOLLOWING:

1. WHERE POSTED SPEEDS ARE 50 MPH OR GREATER, SPACING OF THE PROTECTIVE DEVICES SHALL NOT EXCEED 100 FEET.

2. WHERE POSTED SPEEDS ARE LESS THAN 50 MPH, THE MAXIMUM SPACING OF THE PROTECTIVE DEVICES IN FEET SHALL NOT EXCEED TWICE THE POSTED SPEED IN MILES PER HOUR OR 50 FEET, WHICHEVER SPACING IS GREATER.

THE CONTRACTOR SHALL SCHEDULE THE WORK SO AS TO MINIMIZE THE TIME TRAFFIC IS EXPOSED TO AN ELEVATION DIFFERENCE. ONCE THE CONTRACTOR BEGINS AN ACTIVITY THAT CREATES AN ELEVATION DIFFERENCE, THE ACTIVITY SHALL BE PURSUED AS A CONTINUOUS OPERATION UNTIL THE ELEVATION DIFFERENCE IS ELIMINATED.

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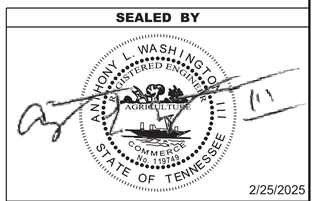


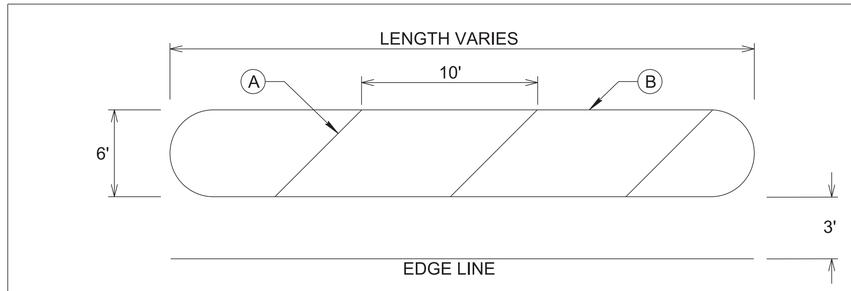
2/25/2025

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT EDGE DROP-OFF NOTES FOR TRAFFIC CONTROL

TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP/HSIP-57(93)	5

S.R. 57 @ L.M. 5.72 (RT.)



PAVEMENT MARKING DETAIL

(A) 12" DIAGONAL TRANSVERSE CHANNELIZATION MARKINGS [ITEM NO. 716-04.04]

(B) 8" SINGLE SOLID WHITE LINE BOUNDARY [ITEM NO. 716-02.04]

PAVEMENT MARKINGS DETAILED ON THIS SHEET SHALL BE WHITE

INFO ONLY

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING DETAILS

SCALE: N.T.S.

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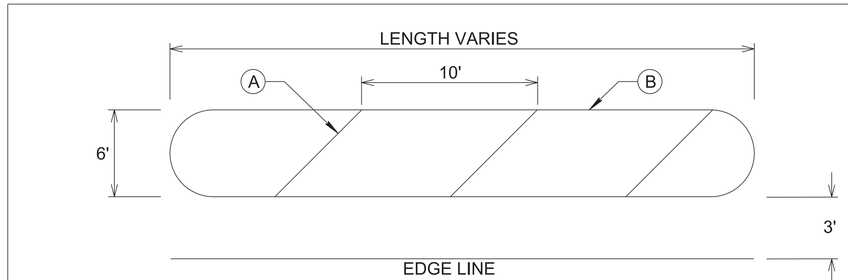
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP/HSIP-57(93)	5A

S.R. 57 @ L.M. 8.01 (LT.)



PAVEMENT MARKING DETAIL

(A) 12" DIAGONAL TRANSVERSE CHANNELIZATION MARKINGS [ITEM NO. 716-04.04]

(B) 8" SINGLE SOLID WHITE LINE BOUNDARY [ITEM NO. 716-02.04]

PAVEMENT MARKINGS DETAILED ON THIS SHEET SHALL BE WHITE

INFO ONLY

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING DETAILS

SCALE: N.T.S.

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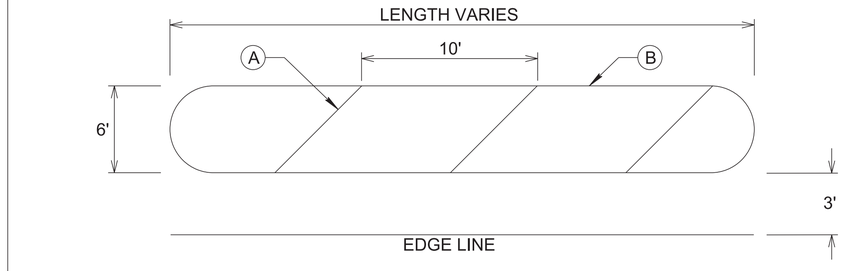
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP/HSIP-57(93)	5B

S.R. 57 @ L.M. 14.35 (LT. & RT.)



PAVEMENT MARKING DETAIL

(A) 12" DIAGONAL TRANSVERSE CHANNELIZATION MARKINGS [ITEM NO. 716-04.04]

(B) 8" SINGLE SOLID WHITE LINE BOUNDARY [ITEM NO. 716-02.04]

PAVEMENT MARKINGS DETAILED ON THIS SHEET SHALL BE WHITE

INFO ONLY

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING DETAILS

SCALE: N.T.S.

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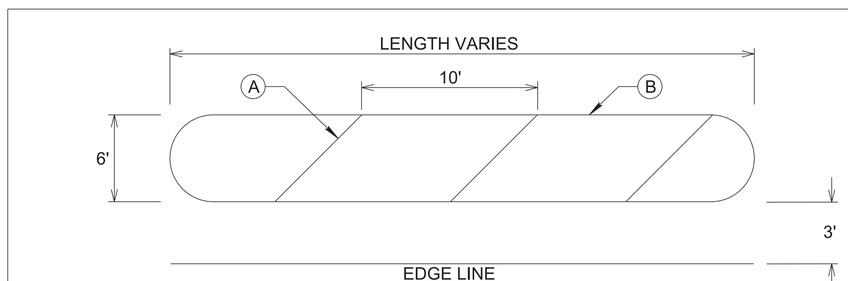
TYPE	YEAR	PROJECT NO.	SHEET NO.
RESURF.	2025	STP/HSIP-57(93)	5C

S.R. 57 @ L.M. 14.37 (LT.)



PAVEMENT MARKING DETAIL

(A) 12" DIAGONAL TRANSVERSE CHANNELIZATION MARKINGS [ITEM NO. 716-04.04]

(B) 8" SINGLE SOLID WHITE LINE BOUNDARY [ITEM NO. 716-02.04]

PAVEMENT MARKINGS DETAILED ON THIS SHEET SHALL BE WHITE

INFO ONLY

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PAVEMENT MARKING DETAILS

SCALE: N.T.S.

1/9/2025
c:\pwworking\east01\d4021297\SR 57 @ LM 14.37.dgn

BRIAN K. EGLI
No. 107196

THIS DOCUMENT HAS BEEN DIGITALLY SIGNED AND SEALED BY:

Brian Egli
2025.01.31 11:24:53 -06'00'

PRINTED COPIES OF THIS DOCUMENT ARE NOT CONSIDERED SIGNED AND SEALED. THE SIGNATURE MUST BE VERIFIED ON THE ELECTRONIC DOCUMENTS.

TENNESSEE DEPARTMENT OF TRANSPORTATION
505 DEADERICK STREET, SUITE 1200
NASHVILLE, TN 37243
BRIAN K. EGLI, P.E. NO. 107196

THE ABOVE NAMED PROFESSIONAL ENGINEER SHALL BE RESPONSIBLE FOR THE FOLLOWING SHEETS IN ACCORDANCE WITH TENN. CODE ANN. §62-2-306.

SHEET NAME	**SHEET NO.**
SIGNATURE SHEET	STRUCTURE-SIGN 1
BRIDGE PLANS	B1 THRU B10

YEAR	PROJECT NO.	SHEET NO.
2025	24S057-M3-002	STRUCTURE-SIGN 1

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SIGNATURE
SHEET

FED. PROJ. NO. STP/HSIP-57(93)

PIN 132486.00

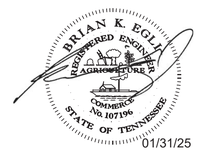
PROJECT NO.	YEAR	SHEET NO.
24S057-M3-002	2025	B1

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

INDEX OF DRAWINGS

INDEX OF REFERENCE DRAWINGS



STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION

INDEX OF DRAWINGS
24-SR57-8.66
OVER
GRISSOM CREEK
24-SR57-9.00
OVER
OVERFLOW
24-SR57-9.36
OVER
STOUT CREEK
24-SR57-10.06
OVER
TEAGUE BRANCH
24-SR57-10.34
OVER
SANDY CREEK
24-SR57-13.44
OVER
WOLF RIVER
BR. NOS. 24SR0570015
24SR0570017
24SR0570019
24SR0570021
24SR0570023
24SR0570025
FAYETTE COUNTY
2025

DESIGNED BY _____ DATE _____
DRAWN BY Z.HAYNES DATE 8/24
SUPERVISED BY K. MARTINKO DATE 8/24
CHECKED BY _____ DATE _____

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10:08:42 AM
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TABULATION OF BRIDGE RELATED WORK AND ESTIMATED QUANTITIES

LOCATION OF BRIDGE AND BRIDGE NUMBER	REFERENCE DRAWINGS TO BE PRINTED WITH CONTRACT DRAWINGS	TYPE OF WORK	604-10.44 EXPANSION JOINT REPAIR L.F.	617-04.01 TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE) S.Y.
24-SR57-8.66 OVER GRISSOM CREEK (24SR0570015)	M-347-87 M-347-90	TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)		708
24-SR57-9.00 OVER OVERFLOW (24SR0570017)	M-347-101 M-347-104	TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)		707
24-SR57-9.36 OVER STOUT CREEK (24SR0570019)	M-369-1 M-369-4	EXPANSION JOINT REPAIR TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	84	879
24-SR57-10.06 OVER TEAGUE BRANCH (24SR0570021)	M-369-17 M-369-20	EXPANSION JOINT REPAIR TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)	89	833
24-SR57-10.34 OVER SANDY CREEK (24SR0570023)	M-369-32 M-369-35	TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)		833
24-SR57-13.44 OVER WOLF RIVER (24SR0570025)	M-353-112 M-353-117	TYPE I THIN EPOXY OVERLAY (EPOXY-URETHANE)		6943
TOTAL			173	10903

PIN 132486.00

EXPANSION JOINT REPAIR NOTES

THE JOINT HEADER SYSTEM SHALL BE FROM QPL 9.002. THE JOINT SEAL SYSTEM CONSISTS OF THE ELASTOMERIC HEADER MATERIAL AND A TWO-PART COLD POUR SILICONE SEALANT. THE CONTRACTOR SHALL ALSO HAVE THE OPTION OF USING A PRECOMPRESSED FOAM WITH SILICONE TOPPED PRODUCT (SIMILAR TO AND INCLUDING BEJS) FROM QPL 7.001 WITH AN APPROPRIATELY SIZED SEAL FOR THE JOINT OPENING, AND AN ELASTOMERIC CONCRETE FROM QPL 9.001. THE SYSTEM SHALL BE INSTALLED UNDER THE DIRECT SUPERVISION OF AN AUTHORIZED TECHNICIAN PROVIDED BY THE SYSTEM MANUFACTURER. THE TECHNICIAN MUST APPROVE ALL ASPECTS OF THE GEOMETRY AND PREPARATION OF THE JOINT LOCATIONS PRIOR TO ANY INSTALLATION OF THE JOINT SYSTEM MATERIALS. PRODUCTS FROM QPL 7.001 "ON A ROLL" AND "COMPRESSION ONLY" WILL NOT BE ALLOWED. THE TOP OF THE QPL 7.001 JOINT FILLER SHALL BE A MINIMUM OF ¾ OF AN INCH BELOW THE ROADWAY SURFACE UNLESS THE MANUFACTURER REUIRES A DEEPER DEPTH.

MANUFACTURERS SPECIFICATIONS AND INSTALLATION PROCEDURES SHALL BE SUBMITTED TO THE PROJECT ENGINEER FOR REVIEW PRIOR TO THE JOINT REPLACEMENT/REPAIR WORK. THE MANUFACTURER AND THE CONTRACTOR SHALL BE RESPONSIBLE FOR THE WORKMANSHIP OF THE JOINT INSTALLATION.

PRIOR TO THE INSTALLATION OF THE NEW JOINT, THE EXISTING JOINT OPENING SHALL BE CLEANED OF ALL DIRT, DEBRIS, AND PRIOR CONSTRUCTION MATERIAL, ETC., THE FULL DEPTH OF THE OPENING. THE SURFACES WHERE THE NEW MATERIAL BONDS TO STAY IN PLACE, SHALL BE CLEANED PER MANUFACTURERS RECOMMENDATION, TO REMOVE ANY SUBSTANCES THAT WOULD INHIBIT BONDING.

THE COST FOR REMOVING THE OLD JOINT SYSTEM, INSTALLING THE NEW JOINT SYSTEM, LABOR, AND ANY MISCELLANEOUS MATERIALS NECESSARY TO INSTALL THE NEW EXPANSION JOINT, IS TO BE INCLUDED UNDER ITEM NUMBER 604-10.44, EXPANSION JOINT REPAIRS, L.F.

TABULATED TRAFFIC CONTROL QUANTITIES

ITEM NUMBER	DESCRIPTION	UNIT	QUANTITY
712-04.01	FLEXIBLE DRUMS (CHANNELIZING)	EACH	155
712-06	SIGNS (CONSTRUCTION)	S.F.	320
713-16.02	TRUCK MOUNTED IMPACT ATTENUATOR (W/ FLASHING ARROW BOARD)	EACH	2

712-06 (CONSTRUCTION) TABULATION

MUTCD CODE	DESCRIPTION	QUANTITY	DIMENSIONS	S.F.	TOTAL
W5-3	ONE LANE BRIDGE	4	4' X4'	16	64
W20-7	FLAGGER (SYMBOL)	4	4' X4'	16	64
W20-4	ROAD WORK 1500 FEET	4	4' X4'	16	64
W20-4	ROAD WORK 1000 FEET	4	4' X4'	16	64
W20-4	ROAD WORK 500 FEET	4	4' X4'	16	64
TOTAL					320

PROJECT NO.	YEAR	SHEET NO.
24S057-M3-002	2025	B2

REVISIONS

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BRIAN K. EGLI REGISTERED ENGINEER AGRICULTURE COMMERCE No. 107196 STATE OF TENNESSEE 01/31/25

STATE OF TENNESSEE

DEPARTMENT OF TRANSPORTATION

BRIDGE TABULATION, ESTIMATED QUANTITIES, AND EXPANSION JOINT REPAIR NOTES

24-SR57-8.66 OVER GRISSOM CREEK
24-SR57-9.00 OVER OVERFLOW
24-SR57-9.36 OVER STOUT CREEK
24-SR57-10.06 OVER TEAGUE BRANCH
24-SR57-10.34 OVER SANDY CREEK
24-SR57-13.44 OVER WOLF RIVER

BR. NOS. 24SR0570015
24SR0570017
24SR0570019
24SR0570021
24SR0570023
24SR0570025

FAYETTE COUNTY
2025

B2

DESIGNED BY _____ DATE _____
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SUPERVISED BY K. MARTINKO DATE 8/24
CHECKED BY _____ DATE _____

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10/09/05 AM
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PIN 132486.00

PROJECT NO.	YEAR	SHEET NO.
24S057-M3-002	2025	B3

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

TYPE 1 THIN EPOXY OVERLAY NOTES :

TYPE 1 THIN EPOXY OVERLAY SYSTEM - USE DECK PRETREATMENT/PRIMER PER MANUFACTURER'S RECOMMENDATION, AND 2 LIFTS OF AN EPOXY-URETHANE COPOLYMER AND AGGREGATE. TYPE 1 OVERLAY SHALL BE APPLIED MECHANICALLY USING METERED EQUIPMENT; HAND MIXING OF MATERIAL IS NOT PERMITTED.

THIN OVERLAY SYSTEM SHALL BE FROM THE QUALIFIED PRODUCTS LIST 23.005 TYPE 1 THIN OVERLAY (EPOXY URETHANE). MINIMUM OVERLAY THICKNESS SHALL BE 3/8 INCH.

APPLICATION EQUIPMENT SHOULD :

A) BE CAPABLE OF METERING, MIXING AND DISTRIBUTING THE POLYMER AND PRETREATMENT TO MANUFACTURER'S RECOMMENDATION.

B) USE AN APPLICATION MACHINE THAT FEATURES POSITIVE DISPLACEMENT VOLUMETRIC METERING PUMPS CONTROLLED BY A HYDRAULIC POWER UNIT.

C) STORE COMPONENTS IN TEMPERATURE CONTROLLED RESERVOIRS CAPABLE OF MAINTAINING 100 DEGREES FAHRENHEIT (PLUS OR MINUS 10 DEGREES) TO INSURE OPTIMAL MIXING.

D) CHECK MIXING RATIO AT THE PUMP OUTLETS AS WELL AS CYCLE COUNTING CAPABILITIES TO MONITOR OUTPUT ON STANDARD FEATURES.

E) USE MOTIONLESS IN-LINE MIXING SO AS TO NOT OVERLY SHEAR THE MATERIAL TO ENTRAP AIR IN THE MIX.

F) MAXIMIZE MATERIAL WORKING TIME BY MIXING IT IMMEDIATELY BEFORE DISPENSING.

AGGREGATE SHALL BE ANGULAR, HAVING LESS THAN 0.2% MOISTURE AND FREE OF DIRT, CLAY, ASPHALT AND OTHER FOREIGN OR ORGANIC MATERIALS. AGGREGATE FOR ALL LAYERS SHALL BE BAUXITE OR FLINT ROCK PRODUCTS FLINT AND MEETS THE FOLLOWING GRADATION:

SIEVE SIZE	% PASSING
NO. 6	95-100
NO. 10	10-35
NO. 20	0-3

FULL AND PARTIAL DEPTH DECK REPAIR SHALL CURE A MINIMUM OF 28 DAYS BEFORE THE OVERLAY IS PLACED. THE 28 DAYS MAY BE WAIVED IF THE OVERLAY MANUFACTURER PROVIDES A METHOD OF TESTING THE REPAIRED AREAS AND APPROVES THE PLACEMENT BY LETTER. TRAFFIC SHALL BE ALLOWED TO USE THE BRIDGE DURING THE CURING PERIOD OF THE PATCHES BUT NOT AFTER SHOTBLASTING. MAGNESIUM PHOSPHATE BASED MATERIALS WILL NOT BE ALLOWED.

THE CONCRETE DECK SURFACE SHALL BE CLEANED BY SHOTBLASTING TO REMOVE ANY OIL, DIRT, RUBBER, TRAFFIC STRIPING, OR ANY OTHER POTENTIAL DETRIMENTAL MATERIAL SUCH AS CURING COMPOUND AND LAITANCES, WHICH THE MANUFACTURER AND ENGINEER'S OPINION WOULD PREVENT PROPER BONDING AND CURING OF THE MATERIAL. IN AREAS WHERE SHOTBLASTING EQUIPMENT CAN NOT REACH (I.E., ALONG CURBS AND BRIDGE RAILS) SANDBLASTING IS PERMITTED TO AN EXTENT TO THE ENGINEER'S AND MANUFACTURER'S APPROVAL. IMMEDIATELY BEFORE APPLICATION, ALL PREPARED SURFACES SHALL BE CLEANED WITH COMPRESSED AIR OR VACUUMED TO REMOVE DUST AND DEBRIS.

ALL SURFACES THAT ARE TREATED SHALL BE DRY AT THE TIME OF APPLICATION. THE OVERLAY SHALL NOT BE APPLIED WHEN IT HAS RAINED 24 HOURS PRIOR TO, OR RAIN IS FORECAST WITHIN 8 HOURS AFTER, APPLICATION. THE MOISTURE CONTENT IN THE DECK SUBSTRATE SHALL BE TESTED. MOISTURE IS NOT TO EXCEED 4.5 PERCENT WHEN MEASURED BY ELECTRONIC METER. IF THE TEST SHOWS EXCESS MOISTURE, THE DECK SHALL CONTINUE TO DRY BEFORE APPLICATION PROCEEDS.

BLUSHING (A WAXY SURFACE COATING ON THE EPOXY) IS CAUSED BY THE REACTION OF MOISTURE WITH THE HARDENING AGENT. BLUSHING CREATES A SURFACE THAT MAKES FUTURE LAYERS DIFFICULT TO ADHERE. LIFTS THAT SHOW SIGNS OF BLUSHING SHALL BE REMOVED AND REPLACED PRIOR TO APPLICATION OF THE NEXT. THE COST TO REMOVE AND REPLACE THESE AREAS SHALL BE AT THE CONTRACTOR'S EXPENSE.

TRAFFIC, OTHER THAN APPLICATION EQUIPMENT, SHALL NOT BE ALLOWED ON ANY PORTION OF THE DECK THAT HAS BEEN SHOTBLASTED OR WHERE PART OF THE APPLICATION HAS BEEN PLACED.

SEE MANUFACTURER'S RECOMMENDATIONS FOR REQUIRED AMBIENT AND SURFACE TEMPERATURES AND HUMIDITY LIMITS FOR APPLICATION.

THE MANUFACTURER SHALL HAVE A REPRESENTATIVE ON THE JOB SITE AT ALL TIMES DURING APPLICATION AND CURE TIME. THE REPRESENTATIVE, ALONG WITH CONSULTATION WITH ENGINEER, MAY SUSPEND ANY ITEM OF WORK THAT IS SUSPECT AND DOES NOT MEET THE REQUIREMENTS OF THE SPECIFICATIONS. WORK SHALL NOT RESUME UNTIL THE ENGINEER AND REPRESENTATIVE ARE SATISFIED THAT APPROPRIATE REMEDIAL ACTION HAS BEEN TAKEN BY THE CONTRACTOR.

ALL COSTS FOR AGGREGATE, EPOXY FOR MINIMUM OF TWO LIFTS, SURFACE PREPARATION, LABOR AND ANY OTHER MISCELLANEOUS MATERIALS REQUIRED TO PLACE THIN OVERLAY SHALL BE INCLUDED IN ITEM NO. 617-04.01, TYPE 1 THIN EPOXY OVERLAY (EPOXY URETHANE), SY.

THICKNESS VERIFICATION: THE PROJECT ENGINEER SHALL BE NOTIFIED OF THE NUMBER OF GALLONS USED ON THE PROJECT WITH NOTARIZED QUANTITY STATEMENTS FROM THE CONTRACTOR AND THE MANUFACTURER. THE CONTRACTOR SHALL VERIFY TO TDOT THAT THE OVERLAY IS AN AVERAGE OF AT LEAST 3/8 INCH THICK AT THREE RANDOM LOCATIONS AGREED UPON BY THE PROJECT ENGINEER AND THE MATERIAL MANUFACTURER REPRESENTATIVE. IF 3/8 INCH AVERAGE IN NOT ACHIEVED, A RETEST SHAL BE PERFORMED IN ADJOINING AREAS. SHALL BE RE-COATED AS DESCRIBED ABOVE BY THE CONTRACTOR AND RE-VERIFIED AT NO ADDITIONAL COST TO TDOT. THIS VERIFICATION MAY CONSIST OF CORES, HOLES, ETC., BUT IN ALL CASES, ANY DESTRUCTIVELY TESTED AREAS SHALL BE REPAIRED BY THE CONTRACTOR BEFORE FINAL ACCEPTANCE BY THE PROJECT ENGINEER.

** SPECIAL NOTE:

THE CONTRACTOR IS TO PREVENT THE TRACKING OF TACKCOAT AND CONSTRUCTION DEBRIS ACROSS THE BRIDGE DECK PRIOR TO APPLICATION OF THE THIN EPOXY OVERLAY.

MILLING THE BRIDGE DECK WILL NOT BE AN OPTION FOR TACKCOAT OR DEBRIS REMOVAL.

REMOVAL SHALL BE AT THE CONTRACTOR'S EXPENSE.

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE 2
COMMERCE
No. 107196
STATE OF TENNESSEE

01/31/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

TYPE I THIN EPOXY OVERLAY NOTES
24-SR57-8.66
OVER
GRISSOM CREEK
24-SR57-9.00
OVER
OVERFLOW
24-SR57-9.36
OVER
STOUT CREEK
24-SR57-10.06
OVER
TEAGUE BRANCH
24-SR57-10.34
OVER
SANDY CREEK
24-SR57-13.44
OVER
WOLF RIVER
BR. NOS. 24SR0570015
24SR0570017
24SR0570019
24SR0570021
24SR0570023
24SR0570025
FAYETTE COUNTY
2025

DESIGNED BY _____ DATE _____
DRAWN BY Z.HAYNES DATE 8/24
SUPERVISED BY K.MARTINKO DATE 8/24
CHECKED BY _____ DATE _____

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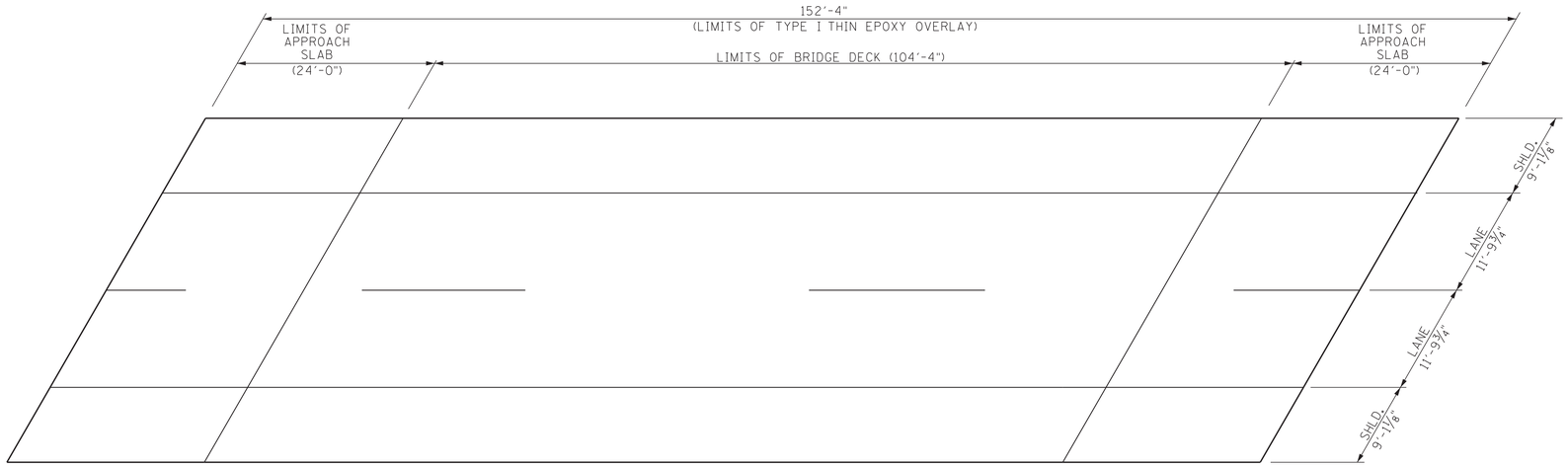
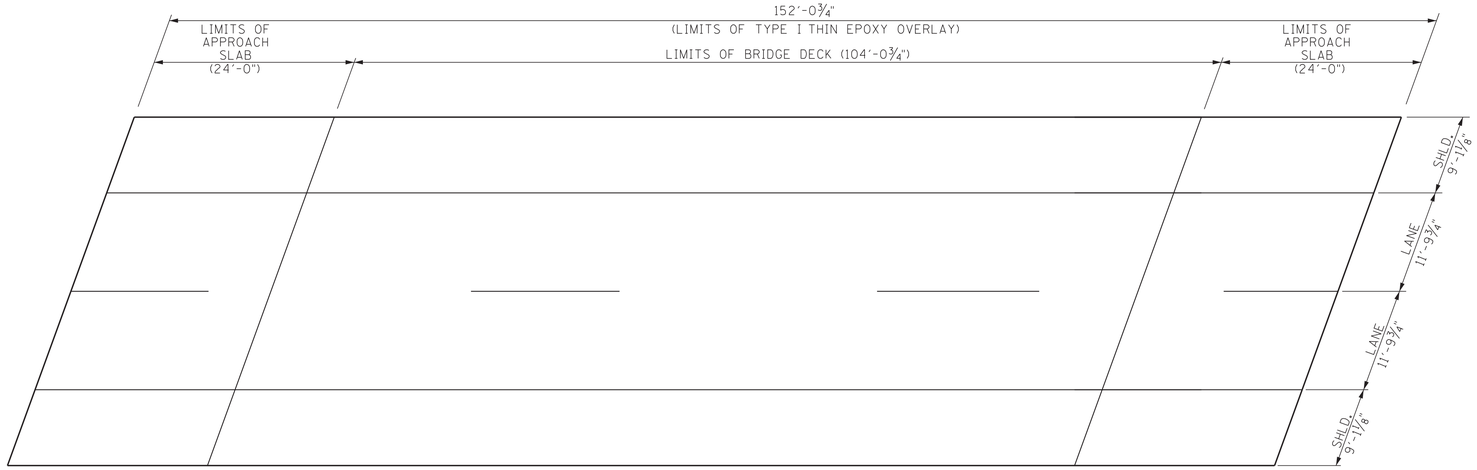


PLAN VIEW

BRIDGE ID. NO. 24SR0570015



PLAN VIEW

BRIDGE ID. NO. 24SR0570017

PIN 132486.00

PROJECT NO.	YEAR	SHEET NO.
24S057-M3-002	2025	B4

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

BRIAN K. EGLI REGISTERED ENGINEER AGRICULTURE COMMERCE No. 107196 STATE OF TENNESSEE

01/31/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PLAN VIEWS
24-SR57-8.66 OVER GRISSOM CREEK
24-SR57-9.00 OVER OVERFLOW
BR. NOS. 24SR0570015 24SR0570017
FAYETTE COUNTY
2025

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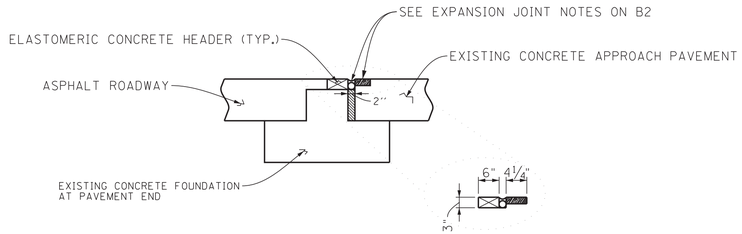
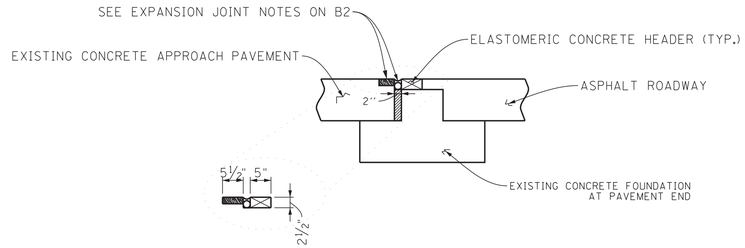


EXPANSION JOINT REPAIR DETAIL @ APPROACH END NO. 1



EXPANSION JOINT REPAIR DETAIL @ APPROACH END NO. 2

PIN 132486.00

PROJECT NO.	YEAR	SHEET NO.
24S057-M3-002	2025	B5

REVISIONS

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PLAN VIEW

BRIDGE ID. NO. 24SR0570019

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE
COMMERCE
No. 107196
STATE OF TENNESSEE
01/31/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
24-SR57-9.36
OVER
STOUT CREEK
BR. NO. 24SR0570019
FAYETTE COUNTY
2025

DESIGNED BY _____ DATE _____
DRAWN BY Z.HAYNES DATE 8/24
SUPERVISED BY K. MARTINKO DATE 8/24
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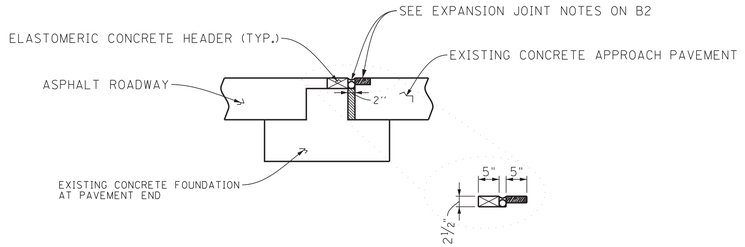

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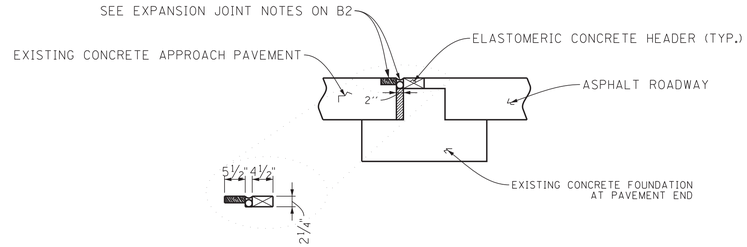


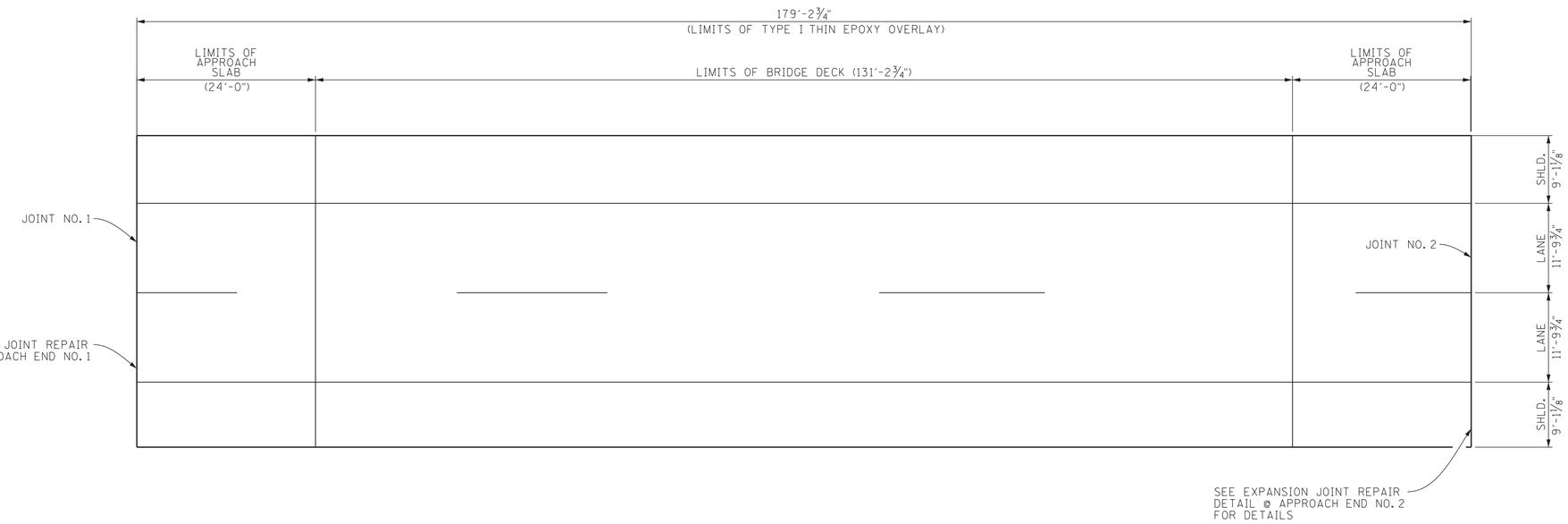
EXPANSION JOINT REPAIR DETAIL @ APPROACH END NO. 2

PIN 132486.00

PROJECT NO.	YEAR	SHEET NO.
24S057-M3-002	2025	B6

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION



PLAN VIEW

BRIDGE ID. NO. 24SR0570021

BRIAN K EGLI REGISTERED ENGINEER AGRICULTURE COMMERCE No. 107196 STATE OF TENNESSEE

01/31/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
24-SR57-10.06
OVER
TEAGUE BRANCH
BR. NO. 24SR0570021
FAYETTE COUNTY
2025

DESIGNED BY		DATE
DRAWN BY	Z.HAYNES	DATE 8/24
SUPERVISED BY	K. MARTINKO	DATE 8/24
CHECKED BY		DATE

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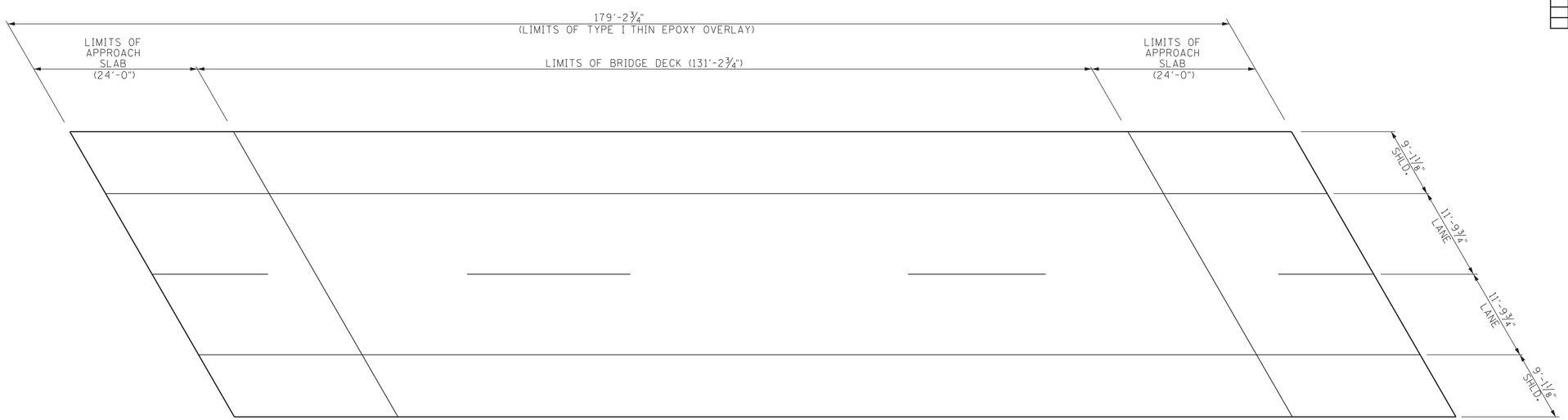
PIN 132486.00

PROJECT NO.	YEAR	SHEET NO.
24S057-M3-002	2025	B7

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION



PLAN VIEW

BRIDGE ID. NO. 24SR0570023

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE
COMMERCE
No. 107196
STATE OF TENNESSEE

01/31/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
24-SR57-10.34
OVER
SANDY CREEK
BR. NO. 24SR0570023
FAYETTE COUNTY
2025

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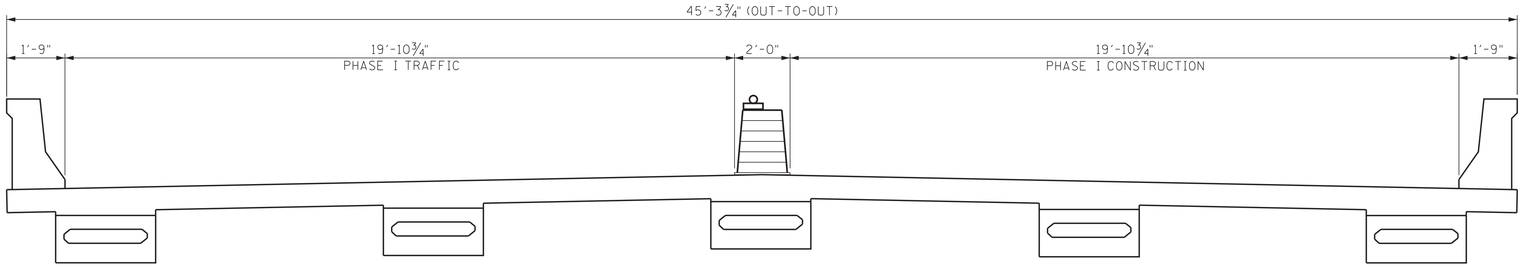
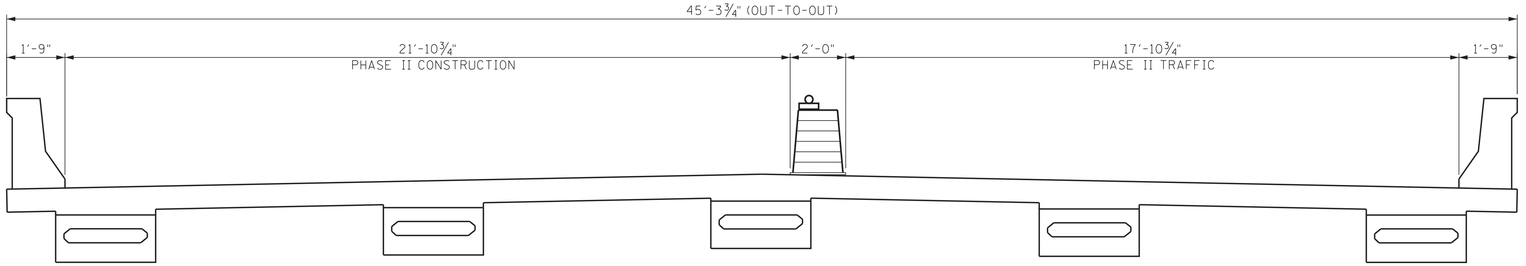


EASTBOUND BRIDGE

(24-SR57-8.66)
(24-SR57-9.00)
(24-SR57-9.36)
(24-SR57-10.06)
(24-SR57-10.34)

PHASE I CONSTRUCTION

(LOOKING AHEAD ON SURVEY)



EASTBOUND BRIDGE

(24-SR57-8.66)
(24-SR57-9.00)
(24-SR57-9.36)
(24-SR57-10.06)
(24-SR57-10.34)

PHASE II CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

BRIAN K. EGLI
No. 107196
01/31/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE CONSTRUCTION
24-SR57-8.66
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GRISSOM CREEK
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FAYETTE COUNTY
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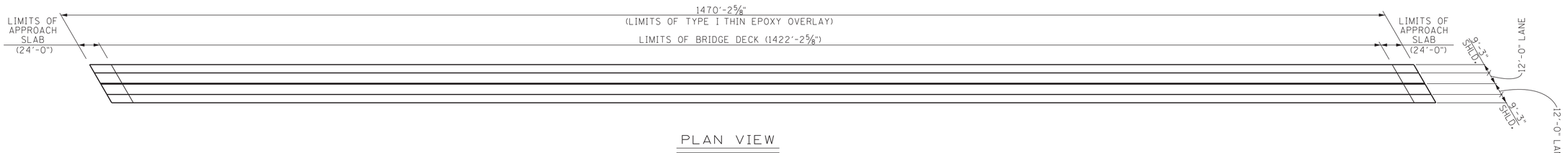
REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION



PLAN VIEW

BRIDGE ID. NO. 24SR0570025

BRIAN K. EGLI
No. 107196
01/31/25

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PLAN VIEW
24-SR57-13.44
OVER
WOLF RIVER
BR. NO. 24SR0570025
FAYETTE COUNTY
2025

DESIGNED BY ______ DATE ______
DRAWN BY Z.HAYNES DATE 8/24
SUPERVISED BY K. MARTINKO DATE 8/24
CHECKED BY ______ DATE ______

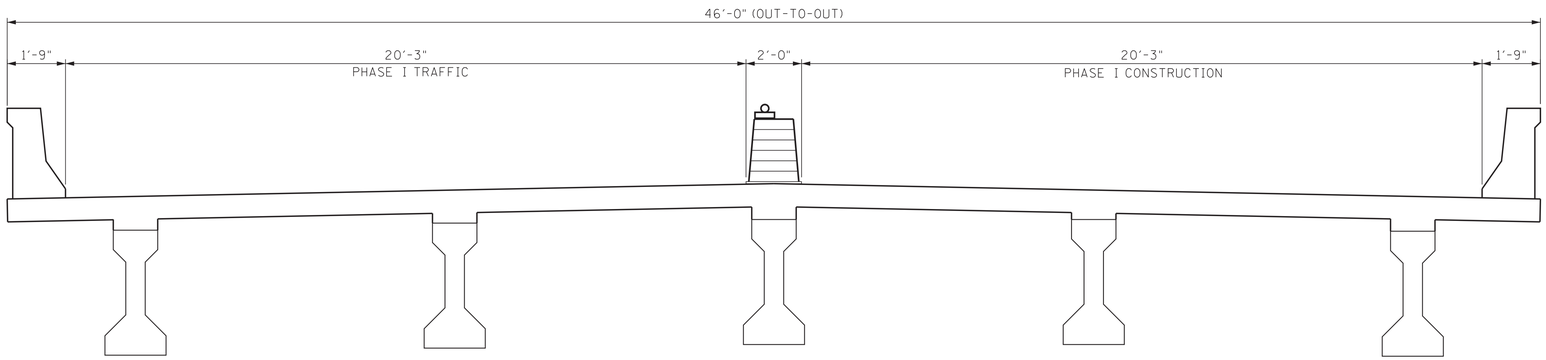
$$$$SYTIME$$$$$$
10:12:09 AM
$$$$DGNSPEC$$$$

PIN 132486.00

PROJECT NO.	YEAR	SHEET NO.
24S057-M3-002	2025	B10

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

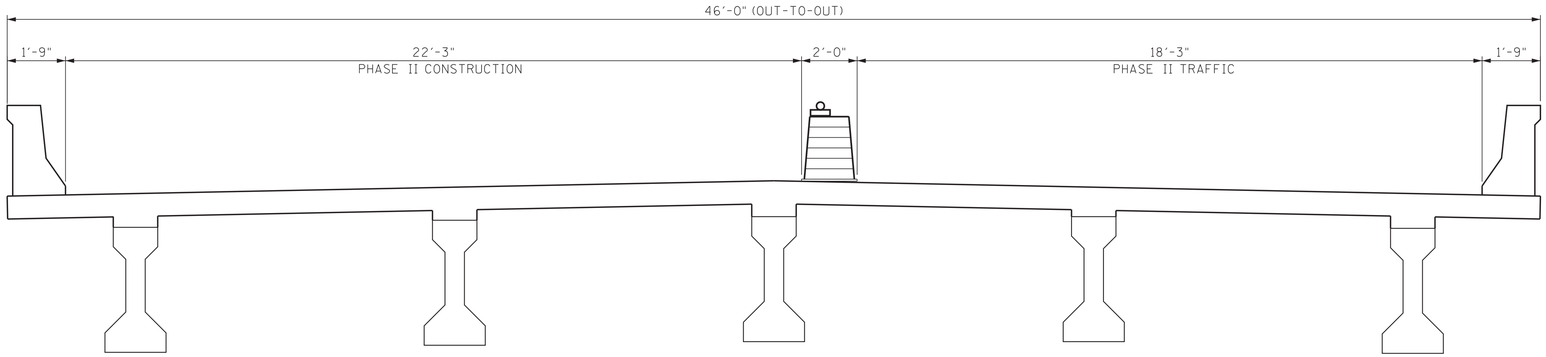


EASTBOUND BRIDGE

(24-SR57-13.44)

PHASE I CONSTRUCTION

(LOOKING AHEAD ON SURVEY)



EASTBOUND BRIDGE

(24-SR57-13.44)

PHASE II CONSTRUCTION

(LOOKING AHEAD ON SURVEY)

BRIAN K. EGLI
REGISTERED ENGINEER
AGRICULTURE
COMMERCE
No. 107196
STATE OF TENNESSEE

01/31/25

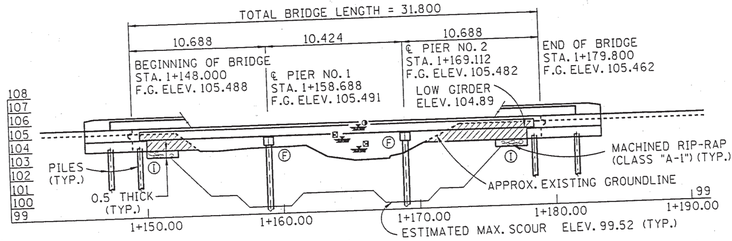
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

PHASE CONSTRUCTION
24-SR57-13.44
OVER
WOLF RIVER
BR. NO. 24SR0570025
FAYETTE COUNTY
2025

DESIGNED BY		DATE	
DRAWN BY	Z.HAYNES	DATE	8/24
SUPERVISED BY	K. MARTINKO	DATE	8/24
CHECKED BY		DATE	

$$$$SYTIME$$$$$$
10:25:13 AM
$$$$DGNSPEC$$$$$$

LAY081.DGN



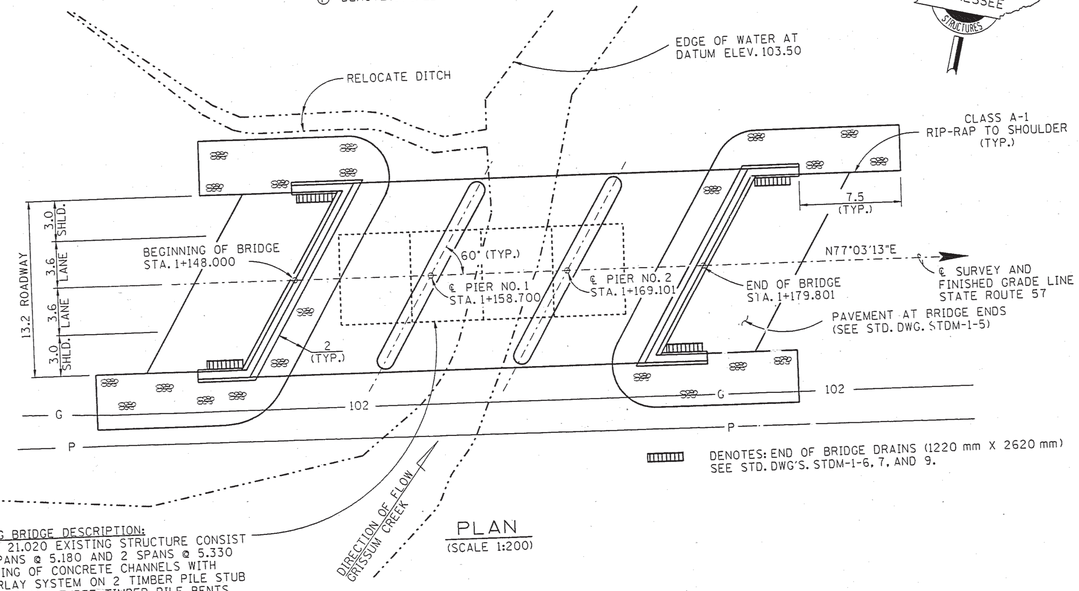
ELEVATION
(SCALE 1:200)

▨ CROSS HATCHED AREA TO BE EXCAVATED AND PAID FOR UNDER ROADWAY ITEM 203M01. (TYP.)

Ⓘ DENOTES: INTEGRAL

Ⓕ DENOTES: FIXED

G DATUM ELEV. 103.50
B 38 YR BRIDGE BACKWATER ELEV. 104.26
D 100 YR BRIDGE BACKWATER ELEV. 105.10



PLAN
(SCALE 1:200)

▭ DENOTES: END OF BRIDGE DRAINS (1220 mm X 2620 mm) SEE STD. DWG'S. STDM-1-6, 7, AND 9.

EXISTING BRIDGE DESCRIPTION:
8.687 X 21.020 EXISTING STRUCTURE CONSIST OF 2 SPANS @ 5.180 AND 2 SPANS @ 5.330 CONSISTING OF CONCRETE CHANNELS WITH AN OVERLAY SYSTEM ON 2 TIMBER PILE STUB ABUTMENTS AND THREE TIMBER PILE BENTS AND BRIDGERAILS. EXISTING BRIDGE NO. (24-57-8.66) TO BE REMOVED.

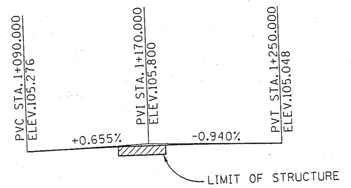
PHASE CONSTRUCTION DESCRIPTION

PHASE I: REMOVE NORTH SIDE OF BRIDGE TO MAINTAIN 3.581 LANE ON SOUTH SIDE OF EXISTING BRIDGE WHICH IS TO BE CONTROLLED BY TEMPORARY TRAFFIC SIGNAL. COMPLETE PHASE I CONSTRUCTION.

PHASE II: TRANSFER TRAFFIC TO NEW CONSTRUCTION MAINTAINING CONTROL BY TEMPORARY TRAFFIC SIGNAL. REMOVE REMAINING EXISTING BRIDGE AND COMPLETE PHASE II CONSTRUCTION.

HYDRAULIC DATA

DRAINAGE AREA = 3657.08 HECTARES
DESIGN DISCHARGE (38 YR.) = 37.00 m^3/s
TOTAL DISCHARGE (38 YR.) = 106.00 m^3/s
WATER AREA PROVIDED BELOW EL. 105.00 = 20.50 m^2
38 YEAR VELOCITY = 1.80 m/s
38 YEAR BRIDGE BACKWATER = 0.00 m @ EL. 105.00
ROADWAY OVERTOPPING EL. = 105.00
100 YEAR DISCHARGE = 123.80 m^3/s @ EL. 105.10
500 YEAR DISCHARGE = 152.10 m^3/s @ EL. 105.30



V.C. = 160

FINISHED GRADE SKETCH (S.R. 57)

CONST. NO. 24006-3238-04

PROJECT NO.	YEAR	SHEET NO.
S.P.	1997	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

LIST OF DRAWINGS

	DWG. NO.	LAST REV. DATE
LAYOUT	M-347-87	
GENERAL NOTES & ESTIMATED QUANTITIES	M-347-88	
FOUNDATION DATA	M-347-89	
SUPERSTRUCTURE	M-347-90	
SUPERSTRUCTURE DETAILS	M-347-91	
SUPERSTRUCTURE DETAILS	M-347-92	
PRESTRESSED BOX BEAM DETAILS	M-347-93	
ABUTMENT NO. 1	M-347-94	
ABUTMENT NO. 1 DETAILS	M-347-95	
ABUTMENT NO. 2 DETAILS	M-347-96	
PIER NO. 1 & NO. 2	M-347-97	
PIER NO. 1 & NO. 2 DETAILS	M-347-98	
FINAL FOUNDATION DATA	M-347-99	
BILL OF STEEL	M-347-100	

LIST OF STANDARD DRAWINGS

	DWG. NO.	LAST REV. DATE
* BRIDGE RAILING CONCRETE PARAPET	STDM-1-1	4-28-97
* STEEL SLIDER PLATE ASSEMBLIES FOR CONCRETE PARAPET AND BRIDGE DECK DRAIN DETAILS	STDM-1-2	6-10-96
	STDM-1-5	4-28-97
* REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STDM-1-6	4-28-97
* BRIDGE END DRAIN DETAILS 2'x8'-7" & 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STDM-1-7	4-28-97
* BRIDGE END DRAIN DETAILS 2'x8'-7" & 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STDM-1-9	6-10-96
* BRIDGE END DRAIN DETAILS 4'x8'-7" WITH PAVEMENT AT BRIDGE ENDS	STDM-4-1	4-28-97
* STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STDM-4-2	6-10-96
* STD. PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STDM-4-3	6-10-96
* STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STDM-4-4	6-10-96
* STD. PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS	STDM-5-1	6-10-96
* STANDARD PILE DETAILS	STDM-5-2	6-10-96
* STANDARD PILE DETAILS	STDM-6-1	6-10-96
* STANDARD SEISMIC DETAILS	STDM-6-2	6-10-96
* STANDARD SEISMIC DETAILS	STDM-9-1	6-10-96
* REINF. BAR SUPPORT DETAILS FOR CONC. SLABS	STDM-10-1	6-10-96
* MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS	STDM-14-3	6-10-96
* STD. DETAILS FOR PRESTRESSED BOX BEAMS		

* DENOTES: THESE DRAWINGS TO BE PRINTED WITH THE PLANS.

LIST OF SPECIAL PROVISIONS

	PROV. NO.	LAST REV. DATE
APPROVAL OF SHOP DRAWINGS	105A	3-6-95

NOTE: ALL DIMENSIONS SHOWN IN METERS UNLESS OTHERWISE NOTED.

DESIGNED BY K. KNOLES (JKZ) DATE 05-96
DRAWN BY J.E. DODSON DATE 05-97
SUPERVISED BY T. TOLLEY (CRB) DATE 05-97
CHECKED BY K. KNOLES DATE 06-97

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE NO. 1
LAYOUT
STATE ROUTE 57
OVER
GRISSUM CREEK
BRIDGE I.D. NO. 24SR0570015
STATION 1+164.000 L.M. 8.66
FAYETTE COUNTY
1997

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-347-87

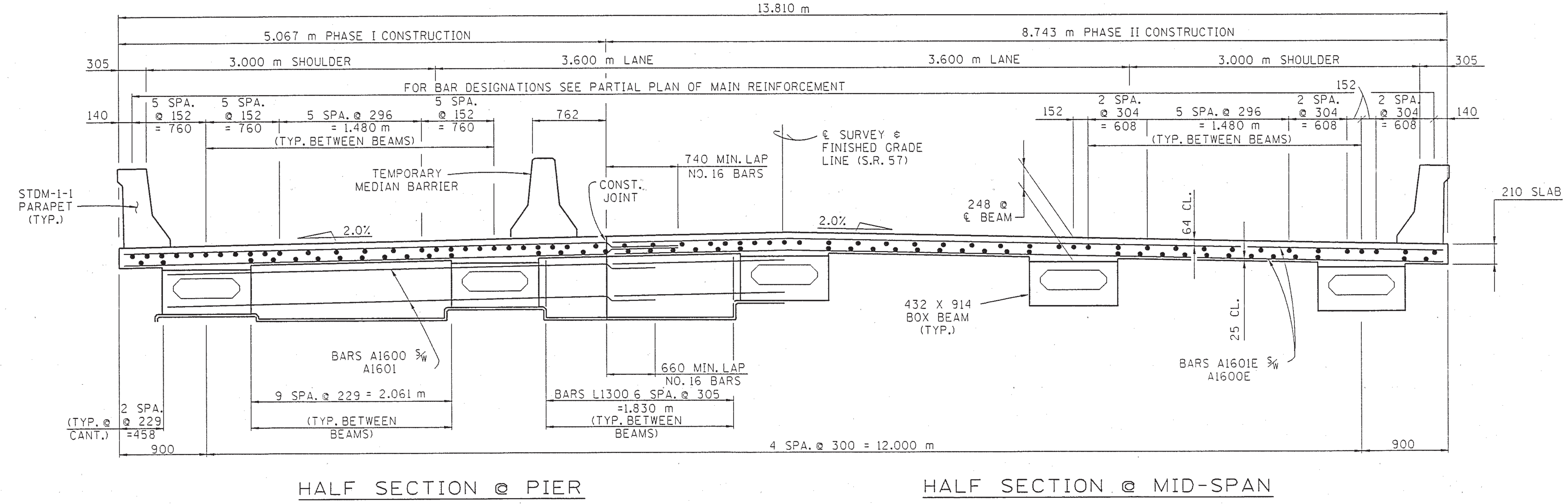
G:\USERS\John\24fayet\grissum
15:36

SUP062.DGN

CONST. NO.	24006-3238-04	
PROJECT NO.	YEAR	SHEET NO.
S.P.	1997	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION



TYPICAL CROSS-SECTION

SUPERSTRUCTURE GENERAL NOTES:

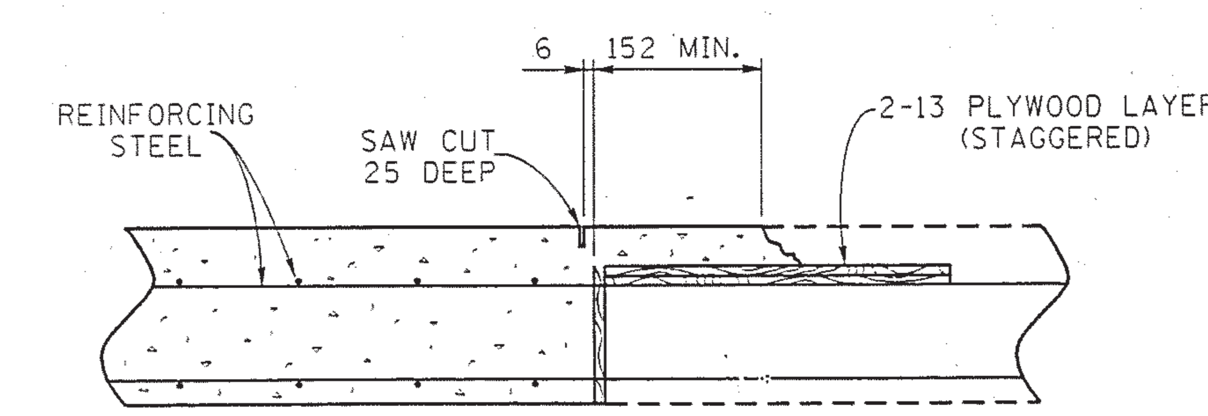
NOTE: NO PORTION OF THE BRIDGERAIL SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

SPECIAL NOTE FOR ANCHOR BOLTS AT BENTS: ANCHOR BOLT ASSEMBLIES AT BENTS SHALL BE IN ACCORDANCE WITH STANDARD DRAWING STDM-6-1.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR BRIDGERAIL. THE BRIDGE RAIL SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO, SEE STANDARD DRAWING STDM-7-1.

NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION.

NOTE: SUPPORT DIAPHRAGMS AT BENTS SHALL BE POURED CONCURRENTLY WITH THE DECK SLAB AND INCLUDED IN THE QUANTITY FOR ITEM 604-03.09.



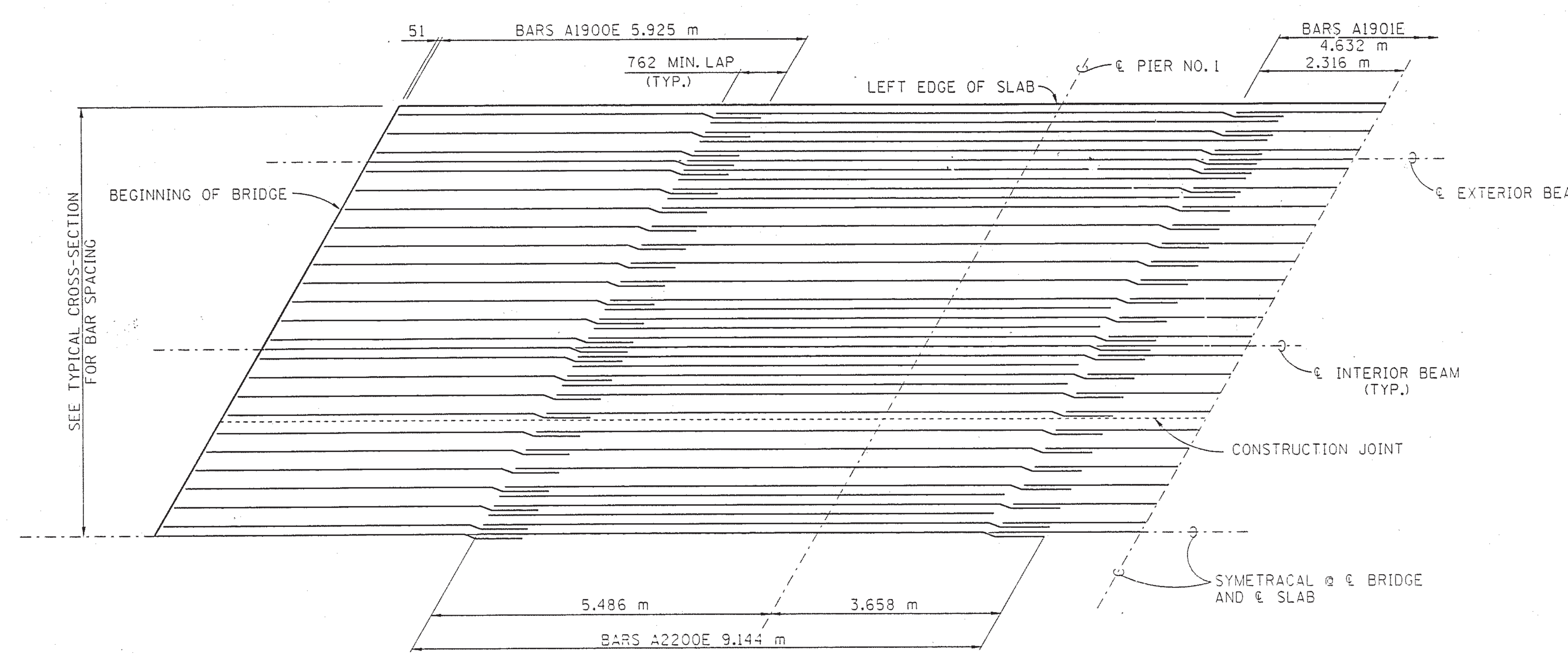
SLAB CONSTRUCTION JOINT DETAIL

DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:

1) NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 3.050 m OR FURTHER THAN 4.572 m FROM AN INTERIOR SUPPORT.

2) THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 4.572 m OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.

CONSTRUCTION JOINTS SHALL BE IN ACCORDANCE WITH THE SLAB CONSTRUCTION JOINT DETAIL SHOWN ABOVE.



PARTIAL PLAN OF MAIN REINFORCEMENT

ESTIMATED QUANTITIES		
CLASS "D" CONCRETE (BRIDGE DECK) m³	EPOXY COATED REINFORCING STEEL kg/m	REINFORCING STEEL kg/m
97	13788	315

METRIC

NOTE: ALL DIMENSIONS SHOWN IN MILLIMETERS, WITH THE EXCEPTION OF STATIONS AND ELEVATIONS, UNLESS OTHERWISE NOTED.

HARDIE M. BROOKS, JR. — STATE OF TENNESSEE

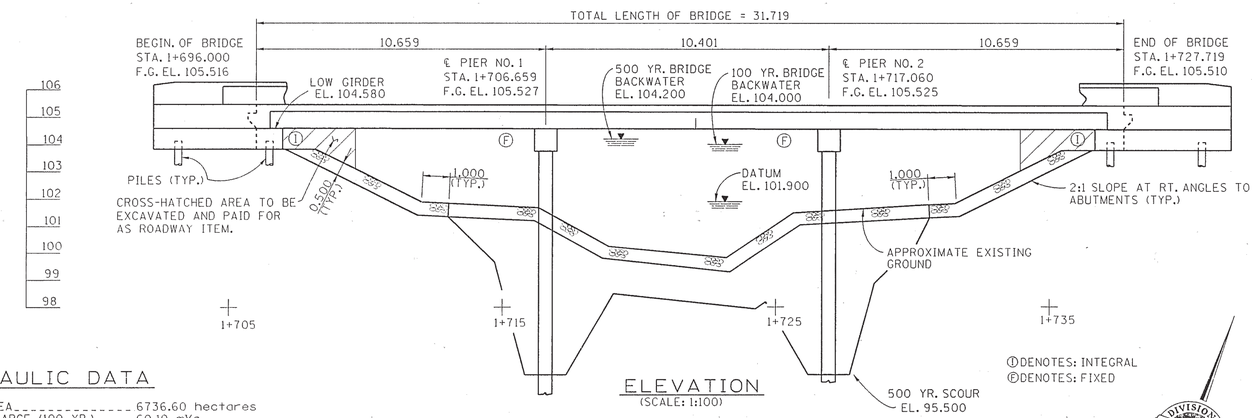
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE NO. 1
SUPERSTRUCTURE
STATE ROUTE 57
OVER
GRISSUM CREEK
STATION 1+164.000
FAYETTE COUNTY
1997

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

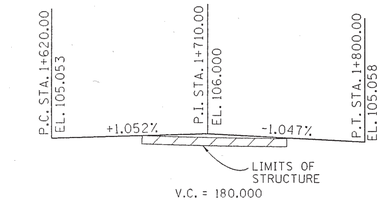
DESIGNED BY K. KNOLES	DATE 05-96
DRAWN BY J. E. DODSON	DATE 04-97
SUPERVISED BY D. HARBISON	DATE 04-97
CHECKED BY K. KNOLES	DATE 04-97

M-347-90

LAY084F.DGN



ELEVATION
(SCALE: 1:100)



FINISHED GRADE SKETCH S.R. 57

CONST. NO. 24006-3238-04

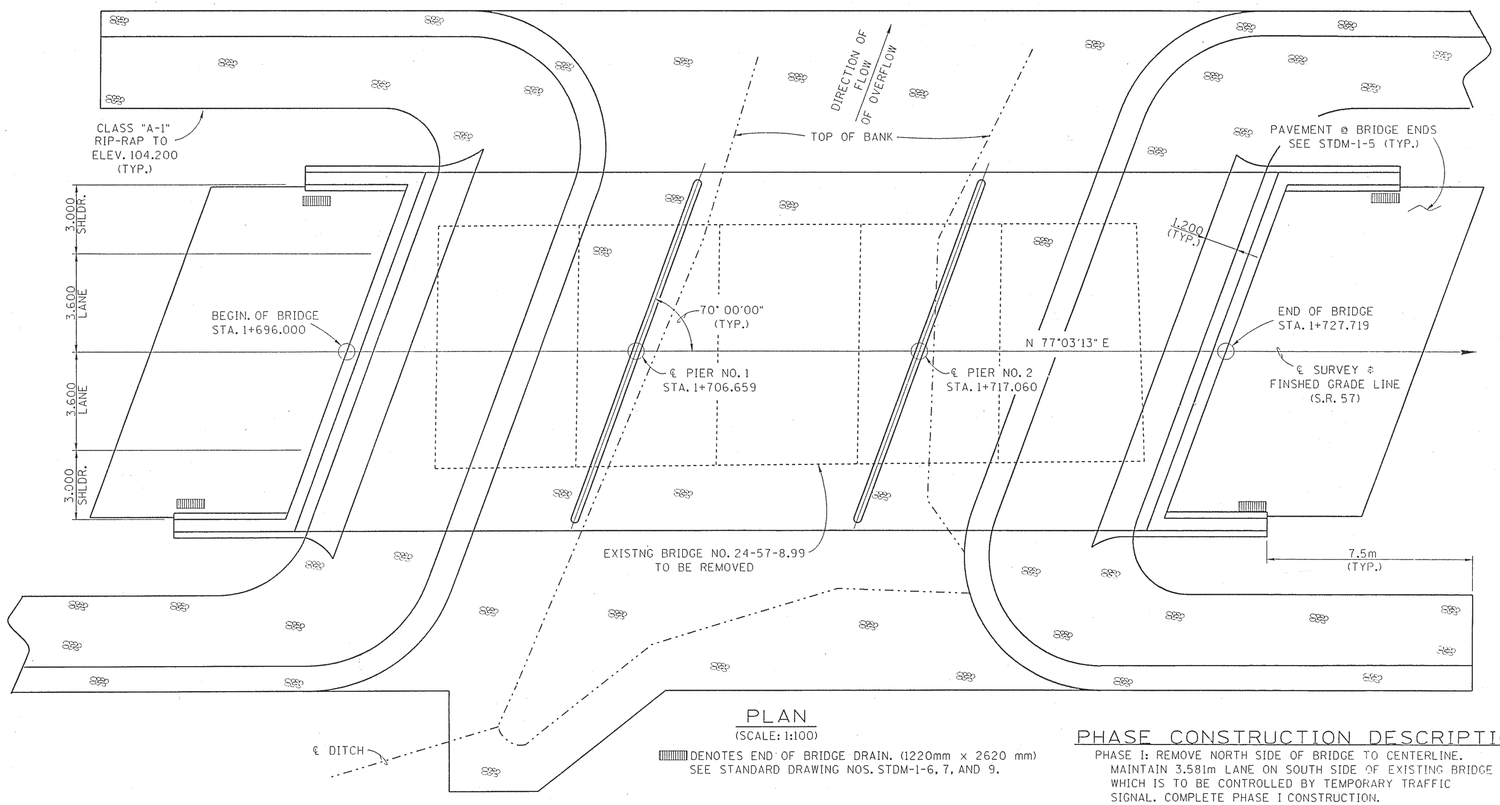
PROJECT NO.	YEAR	SHEET NO.
SP	1997	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

HYDRAULIC DATA

DRAINAGE AREA ..... 6736.60 hectares
DESIGN DISCHARGE (100 YR.) ..... 60.10 m³/s
TOTAL DISCHARGE (100 YR.) ..... 178.00 m³/s
WATERWAY AREA PROVIDED
BELOW EL. 104.00 ..... 49.80 m²
100 YEAR VELOCITY ..... 1.21 mps
100 YEAR BRIDGE BACKWATER ..... 0.10 m @ EL. 104.00 m
ROADWAY OVERTOPPING EL. ..... 104.50 m
500 YEAR DISCHARGE ..... 221 cms @ EL. 104.20 m



PLAN
(SCALE: 1:100)

▥ DENOTES END OF BRIDGE DRAIN. (1220mm x 2620 mm) SEE STANDARD DRAWING NOS. STDM-1-6, 7, AND 9.

PHASE CONSTRUCTION DESCRIPTION

PHASE I: REMOVE NORTH SIDE OF BRIDGE TO CENTERLINE. MAINTAIN 3.581m LANE ON SOUTH SIDE OF EXISTING BRIDGE WHICH IS TO BE CONTROLLED BY TEMPORARY TRAFFIC SIGNAL. COMPLETE PHASE I CONSTRUCTION.

PHASE II: TRANSFER TRAFFIC TO NEW CONSTRUCTION MAINTAINING CONTROL BY TEMPORARY TRAFFIC SIGNAL. REMOVE REMAINING EXISTING BRIDGE AND COMPLETE PHASE II CONSTRUCTION.

LIST OF DRAWINGS

	DWG. NO.	LAST REV. DATE
LAYOUT	M-347-101	
GENERAL NOTES & ESTIMATED QUANTITIES	M-347-102	
FOUNDATION DATA	M-347-103	
SUPERSTRUCTURE	M-347-104	
PRESTRESSED BOX BEAM	M-347-105	
ABUTMENT NO. 1	M-347-106	
ABUTMENT NO. 2	M-347-107	
ABUTMENT NO. 1 & 2 DETAILS	M-347-108	
PIERS NO. 1 AND 2	M-347-109	
FINAL FOUNDATION DATA	M-347-110	
BILL OF STEEL	M-347-111	

LIST OF STANDARD DRAWINGS

	DWG. NO.	LAST REV. DATE
* BRIDGE RAILING CONCRETE PARAPET	STDM-1-1	4-28-97
* STEEL SLIDER PLATE ASSEMBLIES FOR CONCRETE PARAPET AND BRIDGE DECK DRAIN DETAILS	STDM-1-2	6-10-96
* REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STDM-1-5	4-28-97
* BRIDGE END DRAIN DETAILS WITH PAVEMENT AT BRIDGE ENDS	STDM-1-6	4-28-97
* BRIDGE END DRAIN DETAILS WITH PAVEMENT AT BRIDGE ENDS	STDM-1-7	4-28-97
* BRIDGE END DRAIN DETAILS 1220 X 2620 WITH PAVEMENT AT BRIDGE ENDS	STDM-1-9	6-10-96
* STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STDM-4-1	4-28-97
* STD. PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STDM-4-2	6-10-96
* STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STDM-4-3	6-10-96
* STD. PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS	STDM-4-4	6-10-96
* STANDARD PILE DETAILS	STDM-5-1	6-10-96
* STANDARD PILE DETAILS	STDM-5-2	6-10-96
* REINF. BAR SUPPORT DETAILS FOR CONC. SLABS	STDM-9-1	6-10-96
* MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS	STDM-10-1	6-10-96
* STD. DETAILS FOR PRESTRESSED BOX BEAMS	STDM-14-3	6-10-96

* DENOTES: THESE DRAWINGS TO BE PRINTED WITH PLANS.

LIST OF SPECIAL PROVISIONS

	PROV. NO.	LAST REV. DATE
APPROVAL OF SHOP DRAWINGS	105A	3-6-95

NOTE: ALL DIMENSIONS SHOWN IN METERS UNLESS OTHERWISE NOTED.

2017 ADT = 9250
13.2m ROADWAY WITH STDM-1-1 PARAPET
DESIGN SPEED = 100 km/h

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 2
LAYOUT OF BRIDGE
STATE ROUTE 57
OVER
OVERFLOW
BRIDGE I.D. NO. 24SR0570017
STATION 1+711.764
LOG MILE 8.990
FAYETTE COUNTY
1997

DESIGNED BY KEVIN KNOWLES DATE 5-97
DRAWN BY FRED SHARPE DATE 5-97
SUPERVISED BY T. TOLLEY DATE 5-97
CHECKED BY KEVIN KNOWLES DATE 6-97

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-347-101

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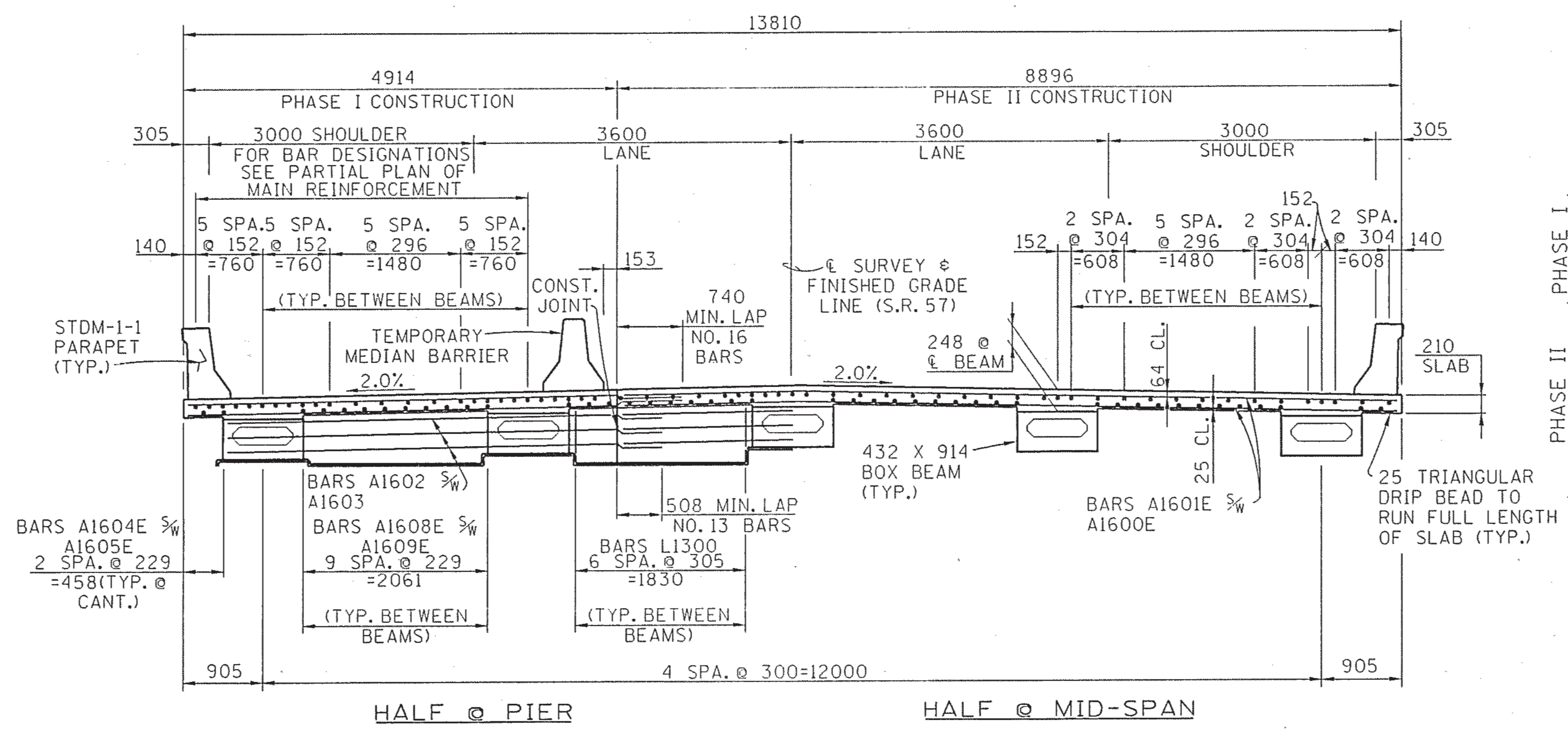
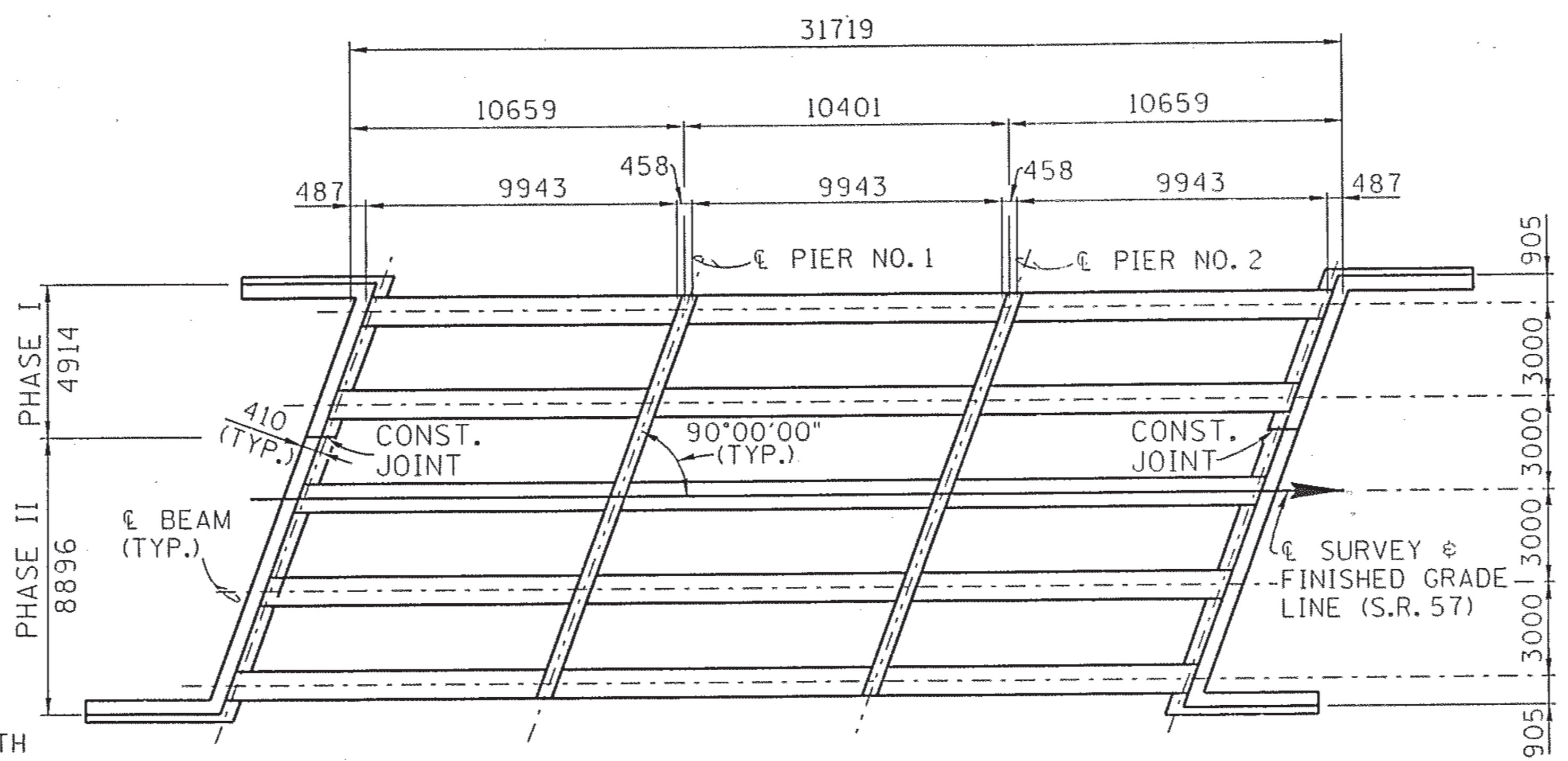
05/16/97
01:16

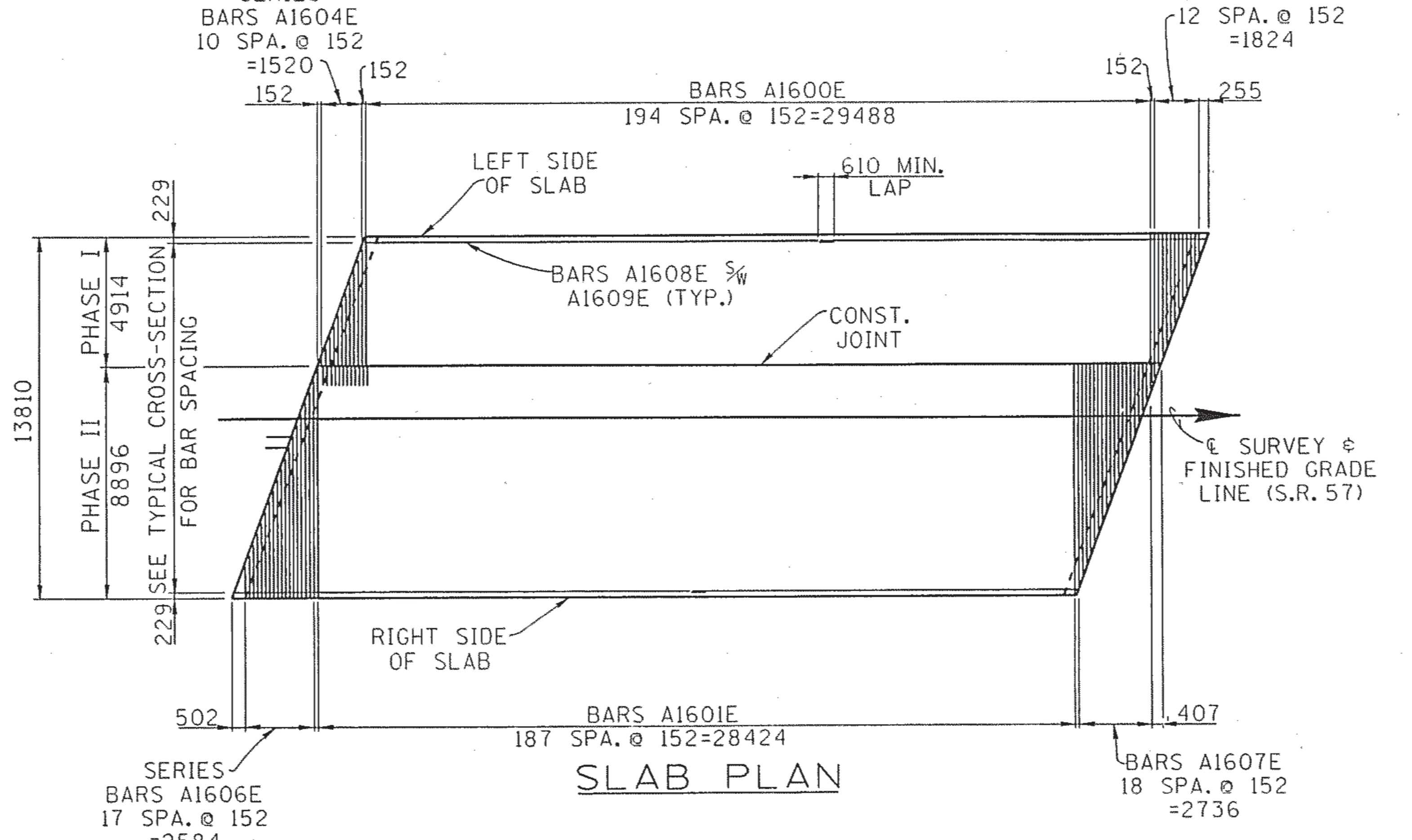
SP136F.DGN



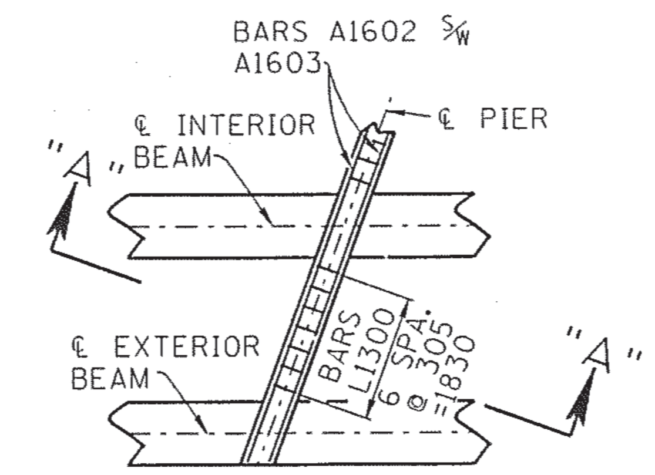
TYPICAL CROSS-SECTION
(LOOKING FORWARD ON SURVEY)



FRAMING PLAN

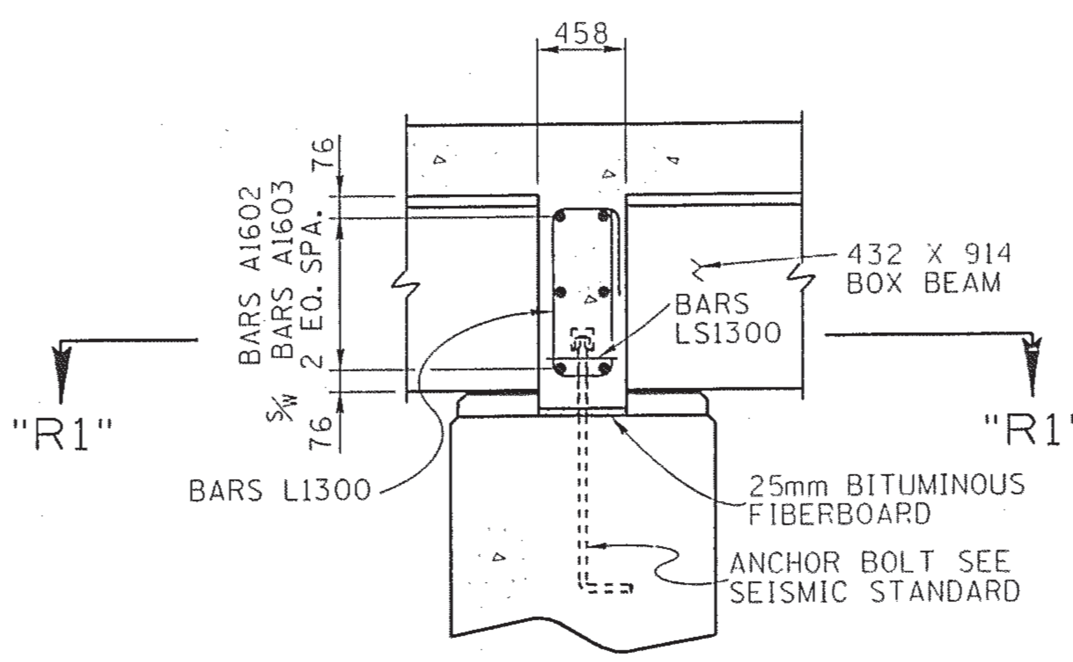


SLAB PLAN



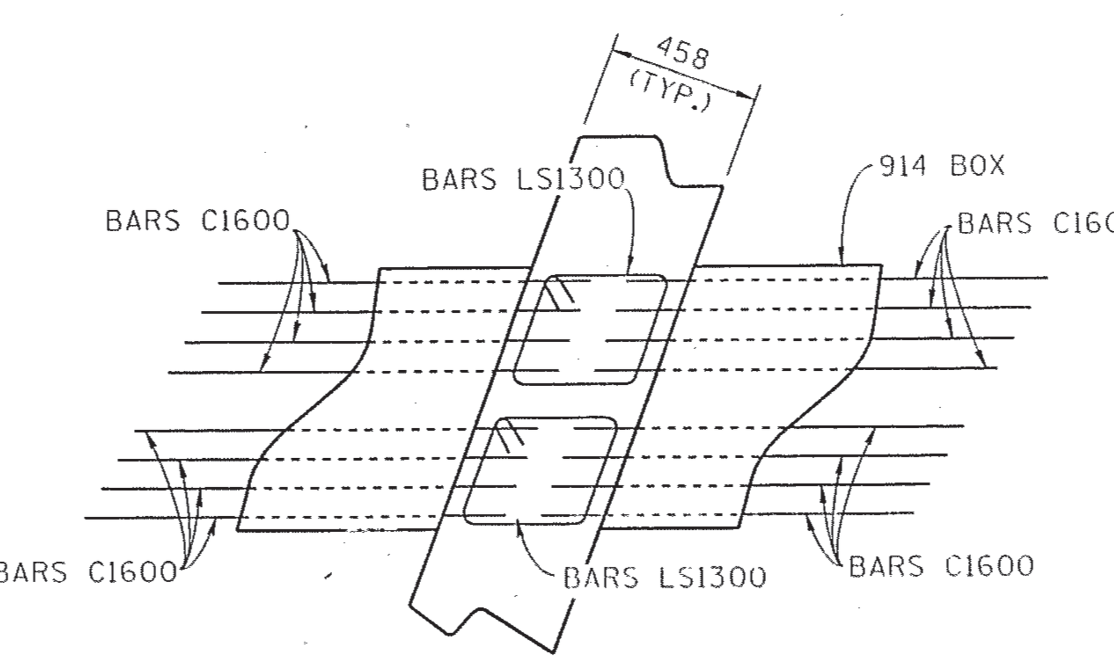
PARTIAL PLAN OF DIAPHRAGM

NOTE: ALL DIMENSIONS SHOWN IN MILLIMETERS, WITH THE EXCEPTION OF STATIONS AND ELEVATIONS, UNLESS OTHERWISE NOTED.



SECTION "A"-"A"

SPECIAL NOTE FOR ANCHOR BOLTS AT BENTS: ANCHOR BOLT ASSEMBLIES AT BENTS SHALL BE IN ACCORDANCE WITH STANDARD DRAWING STDM-6-1.



SECTION R1 - R1

CONST. NO. 24006-3238-04		
PROJECT NO.	YEAR	SHEET NO.
SP	1997	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

SUPERSTRUCTURE GENERAL NOTES:

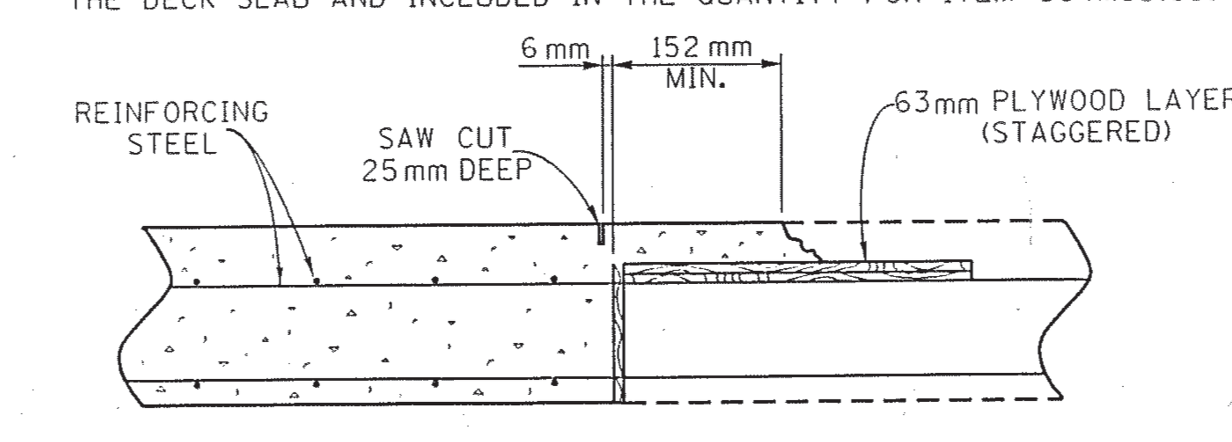
NOTE: NO PORTION OF THE BRIDGERAIL SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

SPECIAL NOTE FOR ANCHOR BOLTS AT BENTS: ANCHOR BOLT ASSEMBLIES AT BENTS SHALL BE IN ACCORDANCE WITH STANDARD DRAWING STDM-6-1.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR BRIDGERAIL. THE BRIDGE RAIL SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO, SEE STANDARD DRAWING STDM-1-1.

NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION.

NOTE: SUPPORT DIAPHRAGMS AT BENTS SHALL BE POURED CONCURRENTLY WITH THE DECK SLAB AND INCLUDED IN THE QUANTITY FOR ITEM 604M03.09.

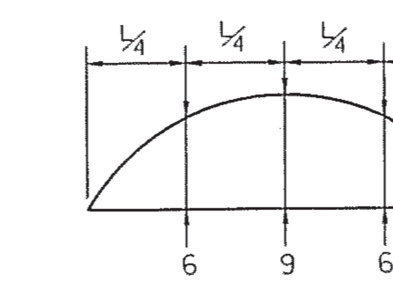


SLAB CONSTRUCTION JOINT DETAIL

DECK CONCRETE POURING SEQUENCE: SLAB CONSTRUCTION JOINTS MAY BE LOCATED AT THE CONTRACTOR'S OPTION SUBJECT TO THE FOLLOWING:

1) NO CONSTRUCTION JOINT MAY BE LOCATED CLOSER THAN 3050mm OR FURTHER THAN 4572mm FROM AN INTERIOR SUPPORT.

2) THE SLAB IN THE MIDDLE SECTION OF BOTH ADJACENT SPANS MUST BE POURED TO WITHIN AT LEAST 4572mm OF THE SUPPORTS EITHER PRIOR TO OR CONCURRENTLY WITH THE SLAB OVER AN INTERIOR SUPPORT.

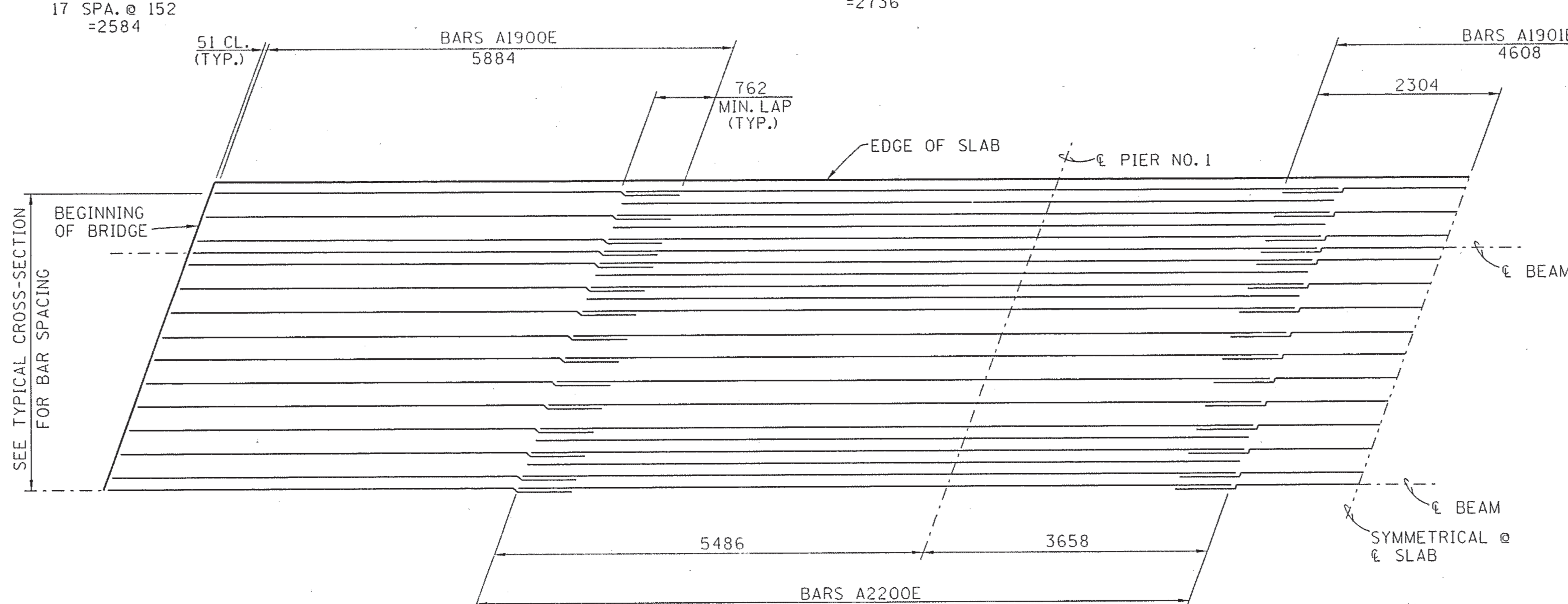


DEAD LOAD CORRECTION CURVE

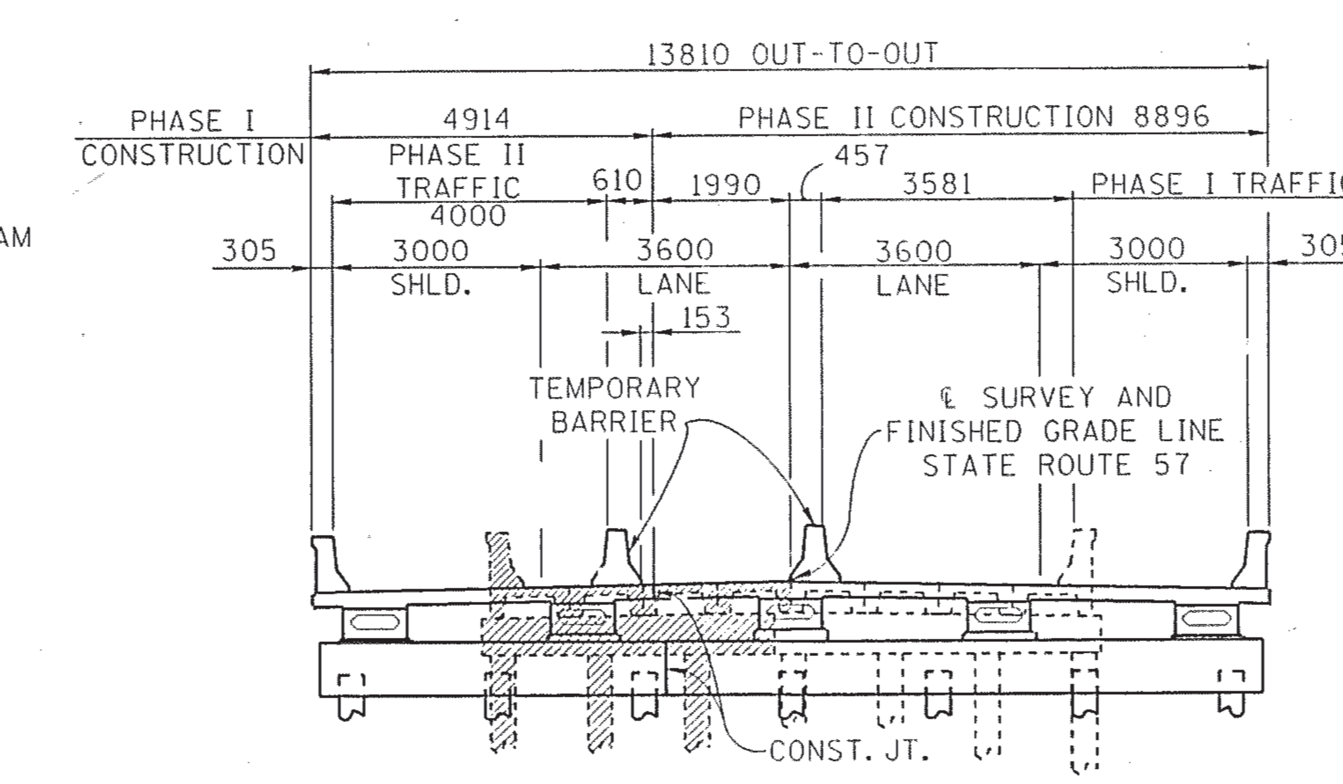
THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER THE SLAB IS IN PLACE.

IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.

ESTIMATED QUANTITIES		
CLASS "D" CONCRETE (BRIDGE DECK) m³	EPOXY COATED REINFORCING STEEL kg	REINFORCING STEEL kg
94	17213	391



PARTIAL PLAN OF MAIN REINFORCEMENT



PHASE CONSTRUCTION SEQUENCE DETAIL
(LOOKING FORWARD ON SURVEY)
▨ DENOTES: PHASE I REMOVAL

DESIGNED BY	KEVIN KNOWLES	DATE	3/97
DRAWN BY	FRED SHARPE	DATE	3/97
SUPERVISED BY	T. TOLLEY	DATE	3/97
CHECKED BY	KEVIN KNOWLES	DATE	6/97

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

HARDIE M. BROOKS, JR. — STATE OF TENNESSEE (seal) — Hardie M. Brooks, Jr. 6/13/97

BRIDGE NO. 2
SUPERSTRUCTURE
STATE ROUTE 57
OVER
OVERFLOW
STATION 1+711.764
LOG MILE 8.990
FAYETTE COUNTY
1997

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-347-104

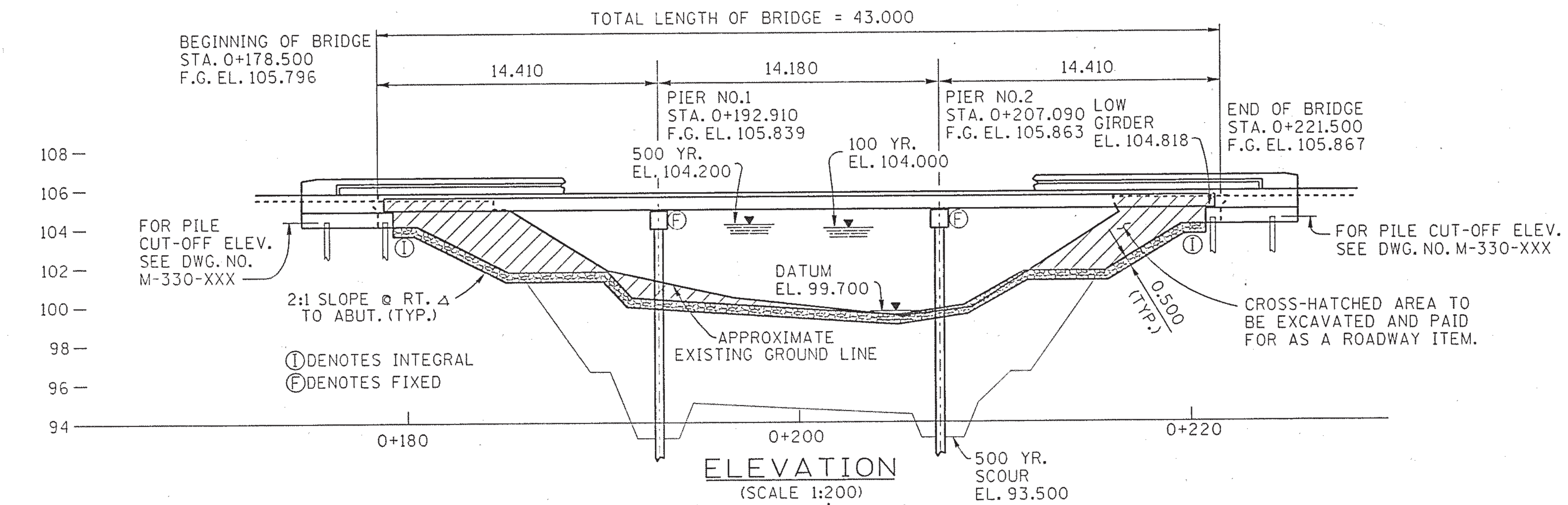
LAY116F.DGN

CONST. NO. 24006-3236-94

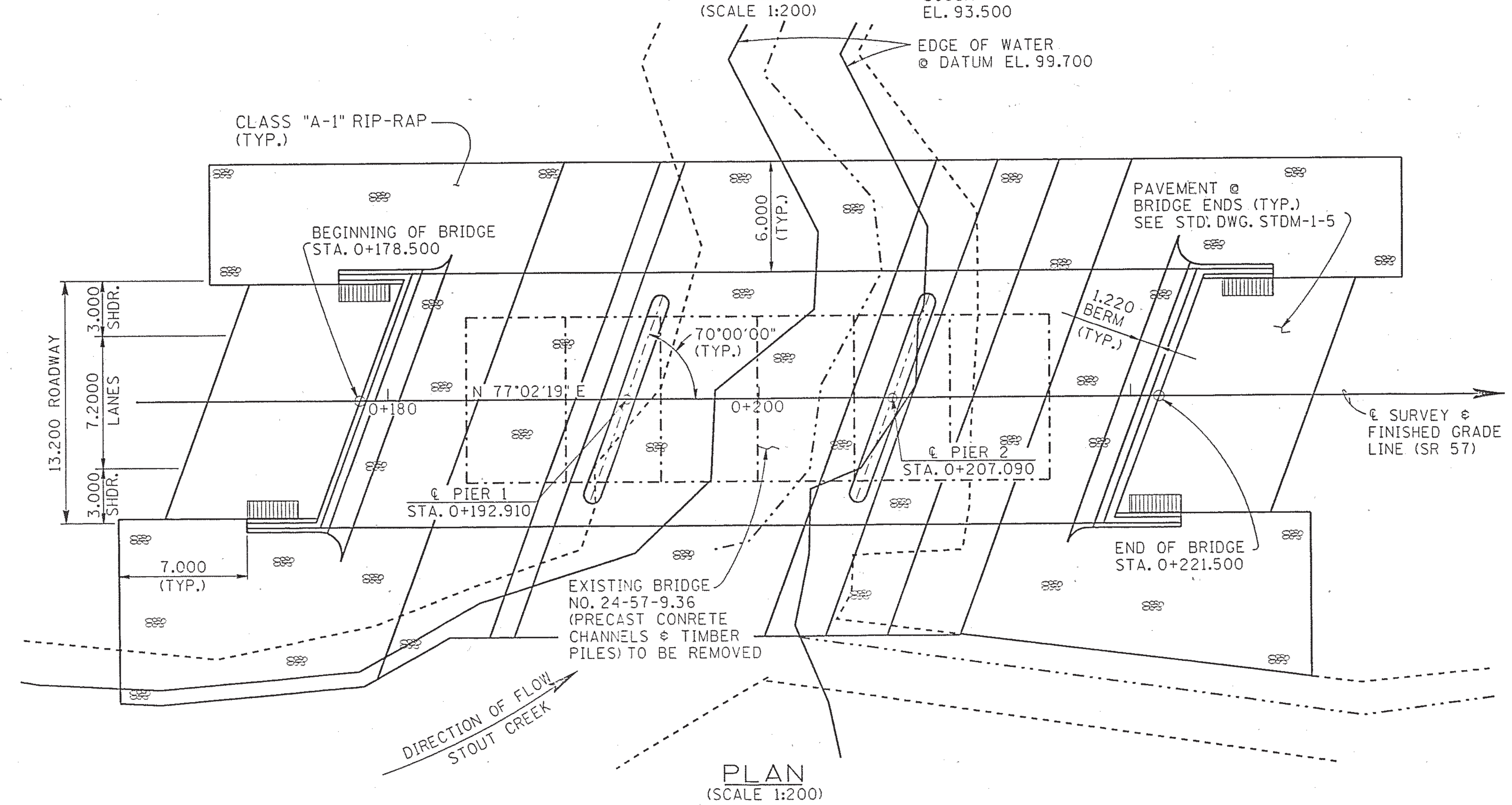
PROJECT NO.	YEAR	SHEET NO.
BR-STP-57(24)	1998	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION
1	8-30-00	RLL	LAST REVISION DATES
2	11-07-01	JEP	LAST REVISION DATES

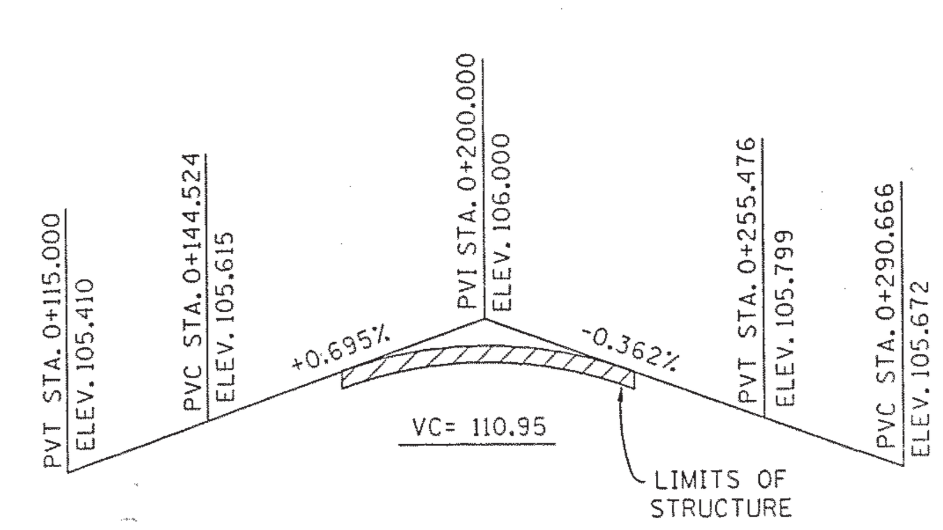


ELEVATION
(SCALE 1:200)



PLAN
(SCALE 1:200)

DENOTES END OF BRIDGE DRAINS SEE STD. DWGS. STDM-1-6, 7 & 8.



GRADE SKETCH

LIST OF DRAWINGS

	DWG. NO.	LAST REV. DATE
LAYOUT OF BRIDGE	M-369-1	11-07-01
GENERAL NOTES & ESTIMATED QUANTITIES	M-369-2	11-07-01
FOUNDATION DATA	M-369-3	
SUPERSTRUCTURE	M-369-4	11-07-01
SUPERSTRUCTURE DETAILS	M-369-5	
SUPERSTRUCTURE DETAILS	M-369-6	
PRESTRESSED BOX BEAM SPANS 1, 2, & 3	M-369-7	
ABUTMENT NO. 1	M-369-8	
ABUTMENT NO. 1 DETAILS	M-369-9	
ABUTMENT NO. 2	M-369-10	
ABUTMENT NO. 2 DETAILS	M-369-11	
BENT NO. 1 & 2	M-369-12	
BENT NO. 1 & 2 DETAILS	M-369-13	
FINAL FOUNDATION DATA	M-369-14	8-30-00
BILL OF STEEL	M-369-15	
BILL OF STEEL	M-369-16	

LIST OF STANDARD DRAWINGS

	DWG. NO.	LAST REV. DATE
BRIDGE RAILING CONCRETE PARAPET	STDM-1-1	4-28-97
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STDM-1-5	4-28-97
BRIDGE END DRAIN DETAILS 610 x 2620 & 1220 x 2620 WITH PAVEMENT AT BRIDGE ENDS	STDM-1-6	4-28-97
BRIDGE END DRAIN DETAILS 610 x 2620 & 1220 x 2620 WITH PAVEMENT AT BRIDGE ENDS	STDM-1-7	4-28-97
BRIDGE END DRAIN DETAILS 610 x 2620 WITH PAVEMENT AT BRIDGE ENDS	STDM-1-8	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STDM-4-1	4-28-97
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STDM-4-2	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STDM-4-3	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS	STDM-4-4	6-10-96
STANDARD PILE DETAILS	STDM-5-1	6-10-96
STANDARD PILE DETAILS	STDM-5-2	6-10-96
STANDARD SEISMIC DETAILS	STDM-6-1	6-10-96
STANDARD SEISMIC DETAILS	STDM-6-2	6-10-96
REINF. BAR SUPPORT DETAILS FOR CONC. SLABS	STDM-9-1	6-10-96
MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS	STDM-10-1	6-10-96
STD. DETAILS FOR PRESTRESSED BOX BEAMS	STDM-14-3	6-10-96

LIST OF SPECIAL PROVISIONS

	PROV. NO.	LAST REV. DATE
APPROVAL OF SHOP DRAWINGS	105A	3-06-95

DIVISION OF STRUCTURES METRIC

2017 ADT = 9800
13.200 ROADWAY WITH STDM-1-1 PARAPET
DESIGN SPEED = 100 km/h

NOTE: ALL DIMENSIONS SHOWN IN METERS UNLESS OTHERWISE NOTED.

DIVISION OF STRUCTURES TENNESSEE

HYDRAULIC DATA

DRAINAGE AREA = 6736.6 hectacres
DESIGN DISCHARGE (100 YR.) = 117 m^3/s
WATER AREA PROVIDED BELOW EL. 104.000 = 88.700 m^2
100 YEAR VELOCITY = 1.32 m/s
100 YEAR BRIDGE BACKWATER = 0.10 m @ EL. 104.000
ROADWAY OVERTOPPING EL. = 104.500
500 YEAR DISCHARGE = 221 m^3/s @ EL. 104.200

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
BRIDGE NO. 1
LAYOUT OF BRIDGE
STATE ROUTE 57
OVER STOUT CREEK
BRIDGE I.D. NO. 24SR0570019
STATION 0+200.000
LOG MILE 9.36
FAYETTE COUNTY
1998

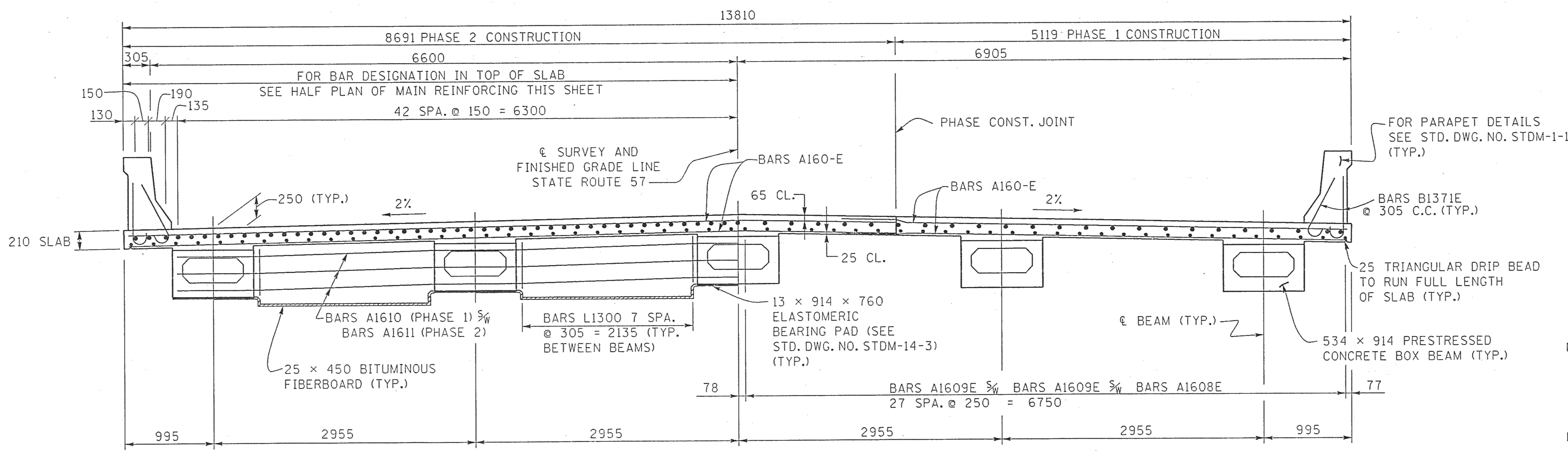
WILLIAM R. WOODS — STATE OF TENNESSEE
William R. Woods
7-23-98

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

DESIGNED BY DAVID WILHITE (J.Z.)	DATE 06-97
DRAWN BY WILHITE \ FOSTER	DATE 04-98
SUPERVISED BY FIELDS & HALL	DATE 04-98
CHECKED BY DAVID WILHITE	DATE 05-98

M-369-1

SP179F.DGN, VI=BORD

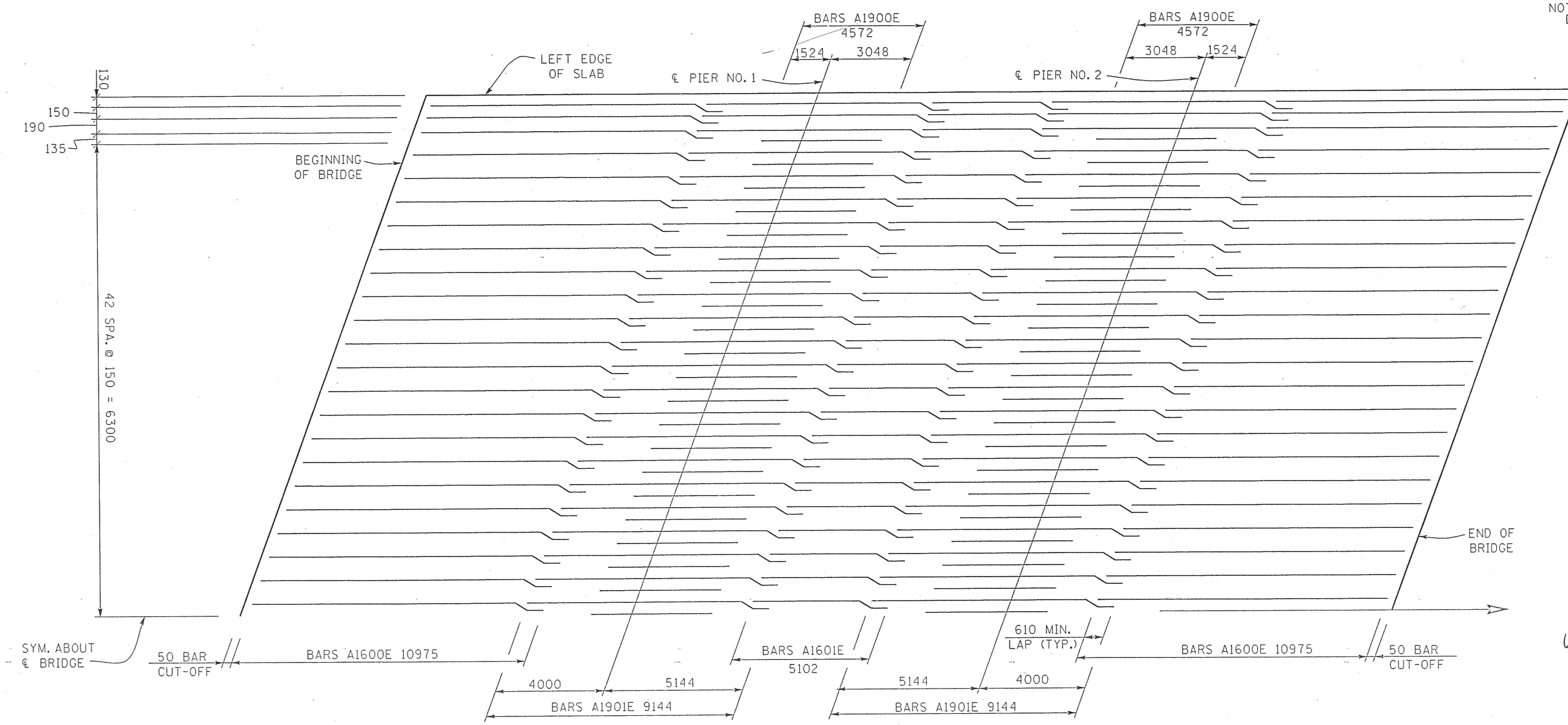


HALF SECTION NEAR BENT

HALF SECTION NEAR MID-SPAN

TYPICAL CROSS-SECTION

LOOKING FORWARD ON SURVEY



HALF PLAN OF MAIN REINFORCING

CONST. NO. 24006-3236-94

PROJECT NO.	YEAR	SHEET NO.
BR-STP-57(24)	1998	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION
1	11-07-01	JEP	REVISE QUANTIES

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. WHEN POURING PARAPET, PROVISIONS SHALL BE MADE FOR SETTING ANCHOR BOLTS FOR PARAPET RAILS. ALSO SEE DRAWING NO. STDM-1-1.

NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED AND THE PERMANENT INTERMEDIATE DIAPHRAGMS BE POURED AND CURED PRIOR TO PLACING ANY LOADS ON THE GIRDERS. HOWEVER, TEMPORARY ERECTION DIAPHRAGMS AND PERMANENT INTERMEDIATE DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

1

ESTIMATED QUANTITIES

CONCRETE CLASS "D" (BRIDGE DECK) M3	EPOXY COATED REINFORCING STEEL KG	REINFORCING STEEL KG
135	18702	416

DIVISION OF STRUCTURES — METRIC

NOTE: ALL DIMENSIONS SHOWN IN MILLIMETERS, WITH THE EXCEPTION OF STATIONS AND ELEVATIONS, UNLESS OTHERWISE NOTED.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 1
SUPERSTRUCTURE
STATE ROUTE 57
OVER STOUT CREEK
STATION 0+200.000
FAYETTE COUNTY
1998

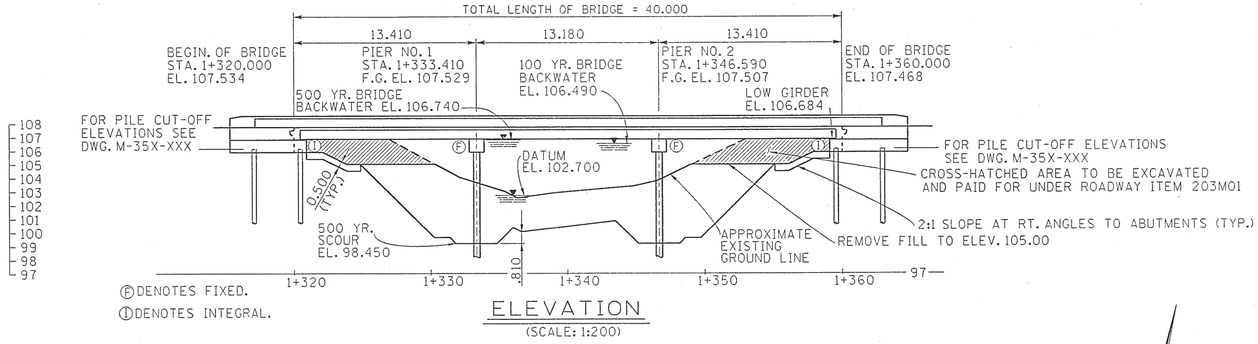
WILLIAM R. WOODS — STATE OF TENNESSEE (seal)
7-23-98

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

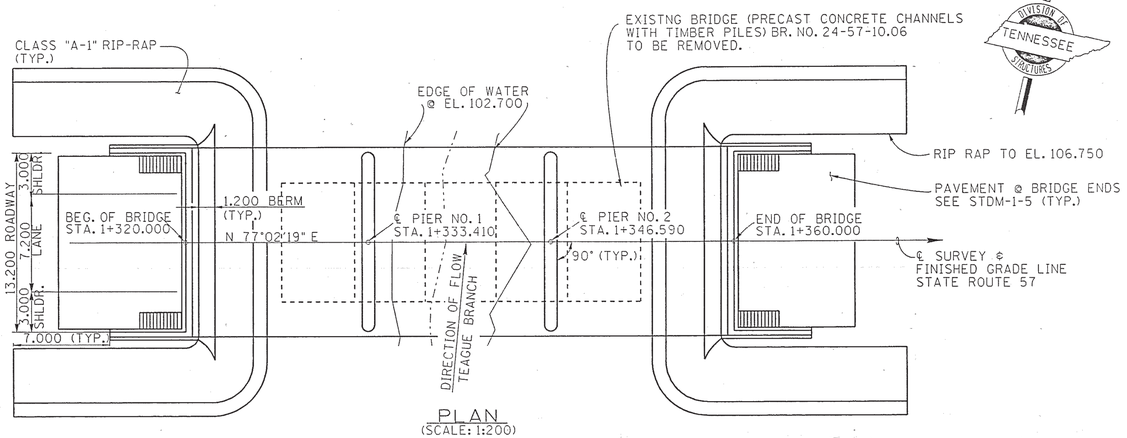
DESIGNED BY	DAVID WILHITE	DATE	9-97
DRAWN BY	KEVIN MARTINKO	DATE	3-98
SUPERVISED BY	FIELDS & HALL	DATE	3-98
CHECKED BY		DATE	

M-369-4

LAY117F.DGN



ELEVATION
(SCALE: 1:200)

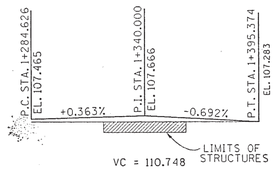


PLAN
(SCALE: 1:200)

▨ DENOTES END OF BRIDGE DRAIN REQUIRED. SEE STD. DWG. NO. STDM-1-6,7 & 8.

HYDRAULIC DATA

DRAINAGE AREA	1743.100 hectares
DESIGN DISCHARGE (100 YR.)	79.600 m³/s
WATERWAY AREA PROVIDED BELOW EL. 106.49	71.800 m²
100 YEAR VELOCITY	1.110 m/s
100 YEAR BRIDGE BACKWATER	0.050 m @ EL. 106.490 m
ROADWAY OVERTOPPING EL.	107.000 m
500 YEAR DISCHARGE	96.600 m³/s @ EL. 106.740 m



GRADE SKETCH

DESIGNED BY	DAVID WILHITE	DATE	07-97
DRAWN BY	D. WILHITE/R. FOSTER	DATE	04-98
SUPERVISED BY	J. FIELDS/G. HALL	DATE	04-98
CHECKED BY	DAVID WILHITE	DATE	05-98

CONST. NO. 24006-3236-94

PROJECT NO.	YEAR	SHEET NO.
BR-STP-57(24)	1998	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION
1	9-6-00	RLL	LAST REVISION DATES

⚠ LIST OF DRAWINGS

	DWG. NO.	LAST REV. DATE
LAYOUT OF BRIDGE	M-369-17	9-6-00
GENERAL NOTES & ESTIMATED QUANTITIES	M-369-18	
FOUNDATION DATA	M-369-19	
SUPERSTRUCTURE	M-369-20	
SUPERSTRUCTURE DETAILS	M-369-21	
SUPERSTRUCTURE DETAILS	M-369-22	
PRESTRESSED BOX BEAM SPANS 1, 2, & 3	M-369-23	
ABUTMENT NO. 1	M-369-24	
ABUTMENT NO. 2	M-369-25	
ABUTMENT NO. 1 & 2 DETAILS	M-369-26	
BENT NO. 1 & 2	M-369-27	
BENT NO. 1 & 2 DETAILS	M-369-28	
FINAL FOUNDATION DATA	M-369-29	9-6-00
BILL OF STEEL	M-369-30	
BILL OF STEEL	M-369-31	

LIST OF STANDARD DRAWINGS

	DWG. NO.	LAST REV. DATE
BRIDGE RAILING CONCRETE PARAPET	STDM-1-1	4-28-97
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STDM-1-5	4-28-97
BRIDGE END DRAIN DETAILS 610 x 2620 & 1220 x 2620 WITH PAVEMENT AT BRIDGE ENDS	STDM-1-6	4-28-97
BRIDGE END DRAIN DETAILS 610 x 2620 & 1220 x 2620 WITH PAVEMENT AT BRIDGE ENDS	STDM-1-7	4-28-97
BRIDGE END DRAIN DETAILS 610 x 2620 WITH PAVEMENT AT BRIDGE ENDS	STDM-1-8	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STDM-4-1	4-28-97
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STDM-4-2	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STDM-4-3	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS	STDM-4-4	6-10-96
STANDARD PILE DETAILS	STDM-5-1	6-10-96
STANDARD PILE DETAILS	STDM-5-2	6-10-96
STANDARD SEISMIC DETAILS	STDM-6-1	6-10-96
STANDARD SEISMIC DETAILS	STDM-6-2	6-10-96
REINF. BAR SUPPORT DETAILS FOR CONC. SLABS	STDM-9-1	6-10-96
MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS	STDM-10-1	6-10-96
STD. DETAILS FOR PRESTRESSED BOX BEAMS	STDM-14-3	6-10-96

LIST OF SPECIAL PROVISIONS

	PROV. NO.	LAST REV. DATE
APPROVAL OF SHOP DRAWINGS	105A	3-06-95

DIVISION OF METRIC STRUCTURES

2017 ADT = 9800
13.200 ROADWAY WITH STDM-1-1 PARAPET
DESIGN SPEED = 100 km/h

NOTE: ALL DIMENSIONS SHOWN IN METERS UNLESS OTHERWISE NOTED.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 2
LAYOUT OF BRIDGE
STATE ROUTE 57
OVER TEAGUE BRANCH
BRIDGE I.D. NO. 24SR057O021
STATION NO. 1+340.000
LOG MILE 10.060
FAYETTE COUNTY
1998

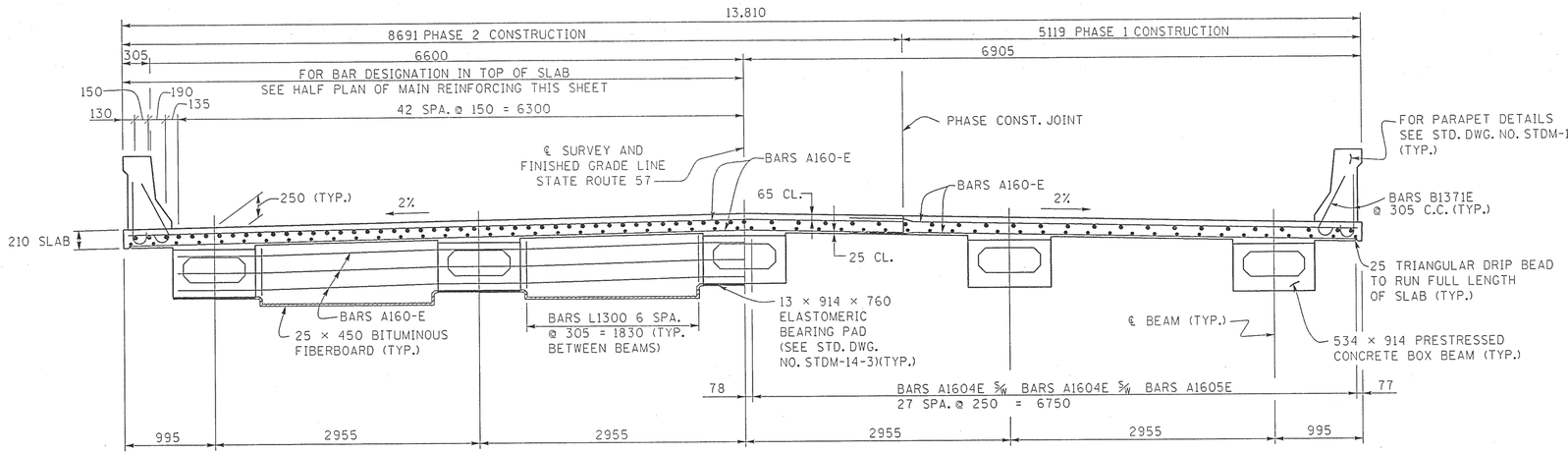
WILLIAM R. WOODS — LICENSED — AGRICULTURE — ENGINEER — STATE OF TENNESSEE

7-23-98

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-369-17

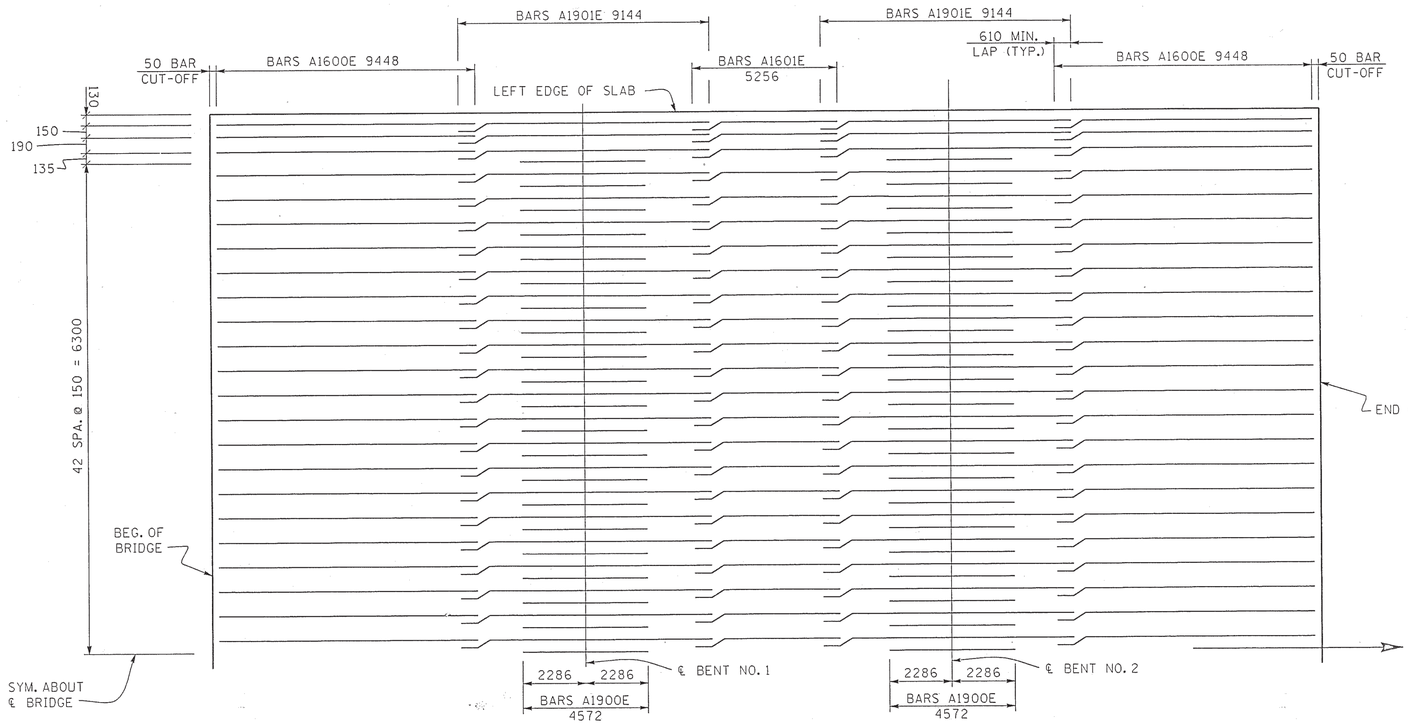
SP180F.DGN, V1=BORD



HALF SECTION NEAR BENT

HALF SECTION NEAR MID-SPAN

TYPICAL CROSS-SECTION



HALF PLAN OF MAIN REINFORCING

DESIGNED BY DAVID WILHITE DATE 9-97
DRAWN BY KEVIN MARTINKO DATE 4-98
SUPERVISED BY FIELDS & HALL DATE 4-98
CHECKED BY DAVID WILHITE DATE 5-98

CONST. NO. 24006-3236-94

PROJECT NO.	YEAR	SHEET NO.
BR-STP-57(24)	1998	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. WHEN POURING PARAPET, PROVISIONS SHALL BE MADE FOR SETTING ANCHOR BOLTS FOR PARAPET RAILS. ALSO SEE DRAWING NO. STDM-1-1.

NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED AND THE PERMANENT INTERMEDIATE DIAPHRAGMS BE POURED AND CURED PRIOR TO PLACING ANY LOADS ON THE GIRDERS. HOWEVER, TEMPORARY ERECTION DIAPHRAGMS AND PERMANENT INTERMEDIATE DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

ESTIMATED QUANTITIES

CONCRETE CLASS "D" (BRIDGE DECK) M3	EPOXY COATED REINFORCING STEEL KG	REINFORC-ING STEEL KG
127	17273	388

DIVISION OF METRIC STRUCTURES

NOTE: ALL DIMENSIONS SHOWN IN MILLIMETERS, WITH THE EXCEPTION OF STATIONS AND ELEVATIONS, UNLESS OTHERWISE NOTED.

WILLIAM R. WOODS — STATE OF TENNESSEE — 7-23-98

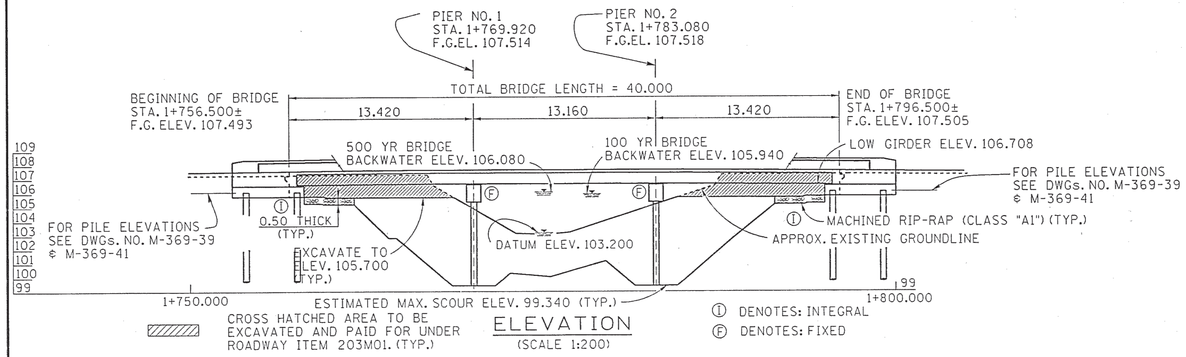
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

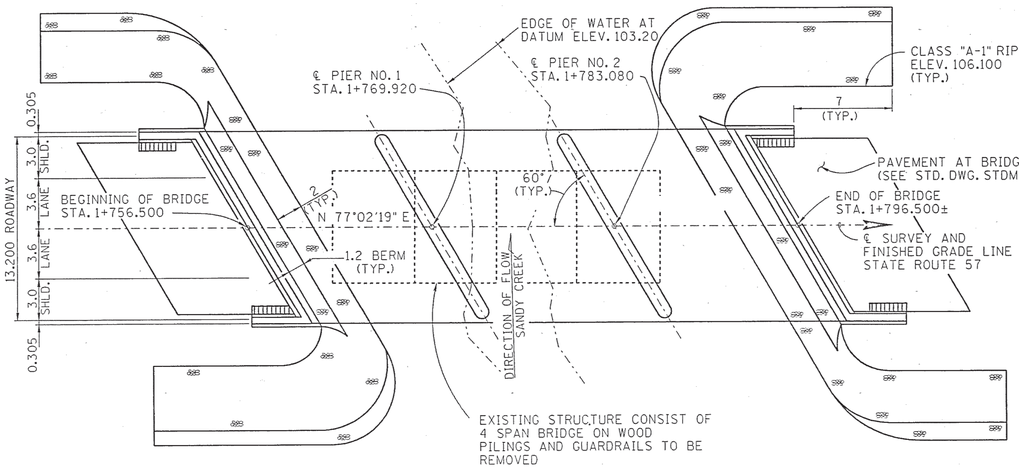
BRIDGE NO. 2
SUPERSTRUCTURE
STATE ROUTE 57
OVER TEAGUE BRANCH
STATION 1+340.000
FAYETTE COUNTY
1998

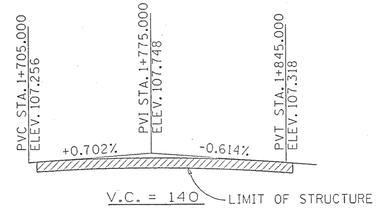
CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-369-20

LAY119F.DGN, VI=BORD







GRADE SKETCH

HYDRAULIC DATA

DRAINAGE AREA = 694.1 HECTRES
DESIGN DISCHARGE (100 YR.) = 46.0 m^3/s
WATER AREA PROVIDED BELOW EL. 105.940 = 30.10 m^2
100 YEAR VELOCITY = 152 m/s
100 YEAR BRIDGE BACKWATER = 0.36 m @ EL. 105.940
ROADWAY OVERTOPPING EL. = 107.000
500 YEAR DISCHARGE = 55.0 m^3/s @ EL. 106.080

DESIGNED BY DAVID WILHITE (J.Z.) DATE 9-97
DRAWN BY WILHITE\CURREY\KKM DATE 4-98
SUPERVISED BY FIELDS & HALL DATE 4-98
CHECKED BY DAVID WILHITE DATE 5-98

CONST. NO. 24006-3236-94

PROJECT NO.	YEAR	SHEET NO.
BR-STP	1998	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION
1	9-7-00	RLL	LAST REVISION DATES

⚠ LIST OF DRAWINGS

	DWG. NO.	LAST REV. DATE
LAYOUT OF BRIDGE	M-369-32	9-07-00
GENERAL NOTES & ESTIMATED QUANTITIES	M-369-33	
FOUNDATION DATA	M-369-34	
SUPERSTRUCTURE	M-369-35	
SUPERSTRUCTURE DETAILS	M-369-36	
SUPERSTRUCTURE DETAILS	M-369-37	
PRESTRESSED BOX BEAM	M-369-38	
ABUTMENT NO. 1	M-369-39	
ABUTMENT NO. 2	M-369-40	
ABUTMENT NO. 1 & 2 DETAILS	M-369-41	
BENT NO. 1 & 2	M-369-42	
BENT NO. 1 & 2 DETAILS	M-369-43	
FINAL FOUNDATION DATA	M-369-44	9-07-00
BILL OF STEEL	M-369-45	
BILL OF STEEL	M-369-46	

LIST OF STANDARD DRAWINGS

	DWG. NO.	LAST REV. DATE
BRIDGE RAILING CONCRETE PARAPET	STDM-1-1	4-28-97
REINFORCED CONCRETE PAVEMENT AT BRIDGE ENDS	STDM-1-5	4-28-97
BRIDGE END DRAIN DETAILS 610x2620 & 1220x2620 WITH PAVEMENT AT BRIDGE ENDS	STDM-1-6	4-28-97
BRIDGE END DRAIN DETAILS 610x2620 & 1220x2620 WITH PAVEMENT AT BRIDGE ENDS	STDM-1-7	4-28-97
BRIDGE END DRAIN DETAILS 610x2620 WITH PAVEMENT AT BRIDGE ENDS	STDM-1-8	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STDM-4-1	4-28-97
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STDM-4-2	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STDM-4-3	6-10-96
STD. PRECAST PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS	STDM-4-4	6-10-96
STANDARD PILE DETAILS	STDM-5-1	6-10-96
STANDARD PILE DETAILS	STDM-5-2	6-10-96
STANDARD SEISMIC DETAILS	STDM-6-1	6-10-96
STANDARD SEISMIC DETAILS	STDM-6-2	6-10-96
REINF. BAR SUPPORT DETAILS FOR CONC. SLABS	STDM-9-1	6-10-96
MISCELLANEOUS ABUTMENT & DRAINAGE DETAILS	STDM-10-1	6-10-96
STD. DETAILS FOR PRESTRESSED BOX BEAMS	STDM-14-3	6-10-96

LIST OF SPECIAL PROVISIONS

	PROV. NO.	LAST REV. DATE
APPROVAL OF SHOP DRAWINGS	105A	3-06-95

DIVISION OF TENNESSEE STRUCTURES

DIVISION OF METRIC STRUCTURES

WILLIAM R. WOODS — STATE OF TENNESSEE
7-23-98

2017 ADT = 9800
13.200 ROADWAY WITH STDM-1-1 PARAPET
DESIGN SPEED = 100 km/h

NOTE: ALL DIMENSIONS SHOWN IN METERS UNLESS OTHERWISE NOTED.

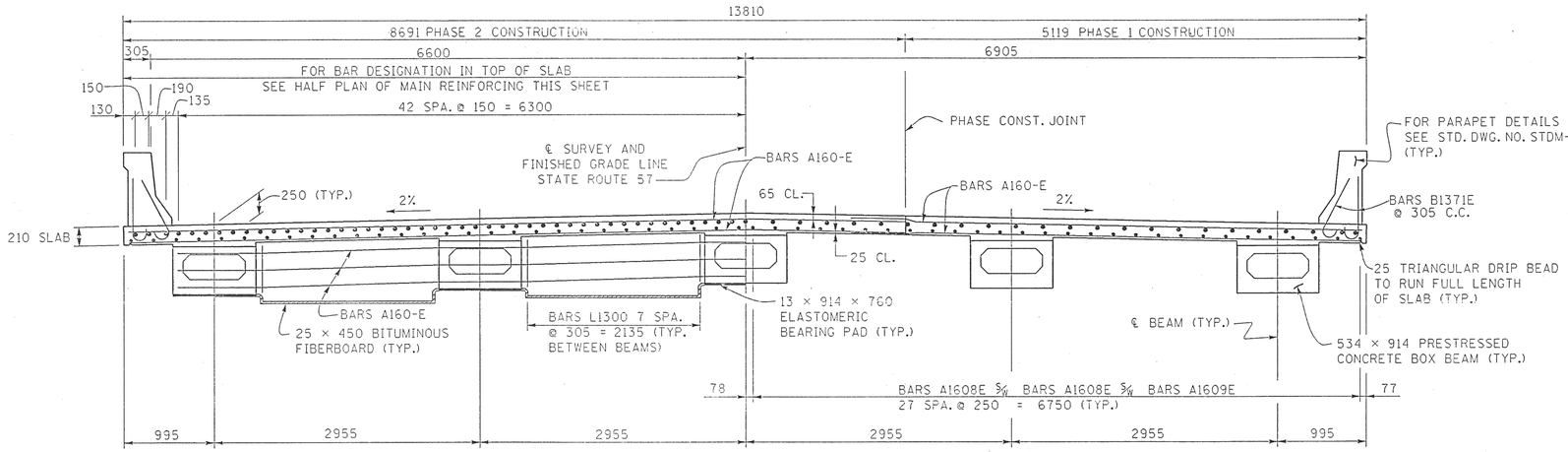
STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 3
LAYOUT OF BRIDGE
STATE ROUTE 57
OVER SANDY CREEK
BRIDGE I.D. NO. 24SR0570023
STATION 1+776.500 L.M. 10.34
FAYETTE COUNTY
1997

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

M-369-32

SP181F.DGN. VI=BORD

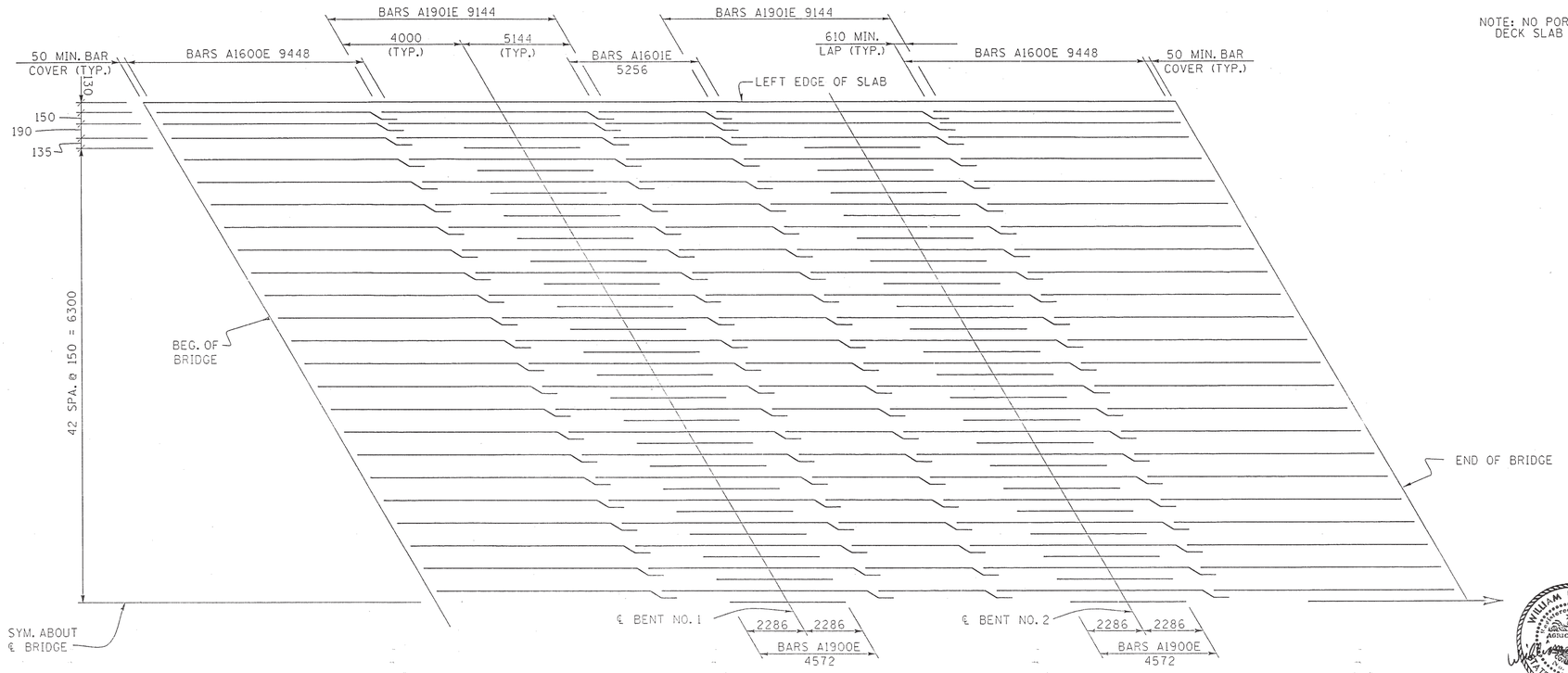


HALF SECTION NEAR BENT

HALF SECTION NEAR MID-SPAN

TYPICAL CROSS-SECTION

(LOOKING FORWARD ON SURVEY)



HALF PLAN OF MAIN REINFORCING

DESIGNED BY DAVID WILHITE DATE 9-97
DRAWN BY KEVIN MARTINKO DATE 4-98
SUPERVISED BY FIELDS & HALL DATE 4-23
CHECKED BY DAVID WILHITE DATE 5-98

CONST. NO. 24006-3236-94

PROJECT NO.	YEAR	SHEET NO.
BR-STP-57(24)	1998	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. WHEN POURING PARAPET, PROVISIONS SHALL BE MADE FOR SETTING ANCHOR BOLTS FOR PARAPET RAILS. ALSO SEE DRAWING NO. STDM-1-1.

NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED AND THE PERMANENT INTERMEDIATE DIAPHRAGMS BE POURED AND CURED PRIOR TO PLACING ANY LOADS ON THE GIRDERS. HOWEVER, TEMPORARY ERECTION DIAPHRAGMS AND PERMANENT INTERMEDIATE DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.

NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

ESTIMATED QUANTITIES

CONCRETE CLASS "D" (BRIDGE DECK) M3	EPOXY COATED REINFORCING STEEL KG	REINFORCING STEEL KG
122	17187	441

DIVISION OF METRIC STRUCTURES

NOTE: ALL DIMENSIONS SHOWN IN MILLIMETERS, WITH THE EXCEPTION OF STATIONS AND ELEVATIONS, UNLESS OTHERWISE NOTED.

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

BRIDGE NO. 3
SUPERSTRUCTURE
STATE ROUTE 57
OVER SANDY CREEK
STATION 1+776.500
FAYETTE COUNTY
1998

WILLIAM R. WOODS — REGISTERED ENGINEER — AGRICULTURE — STATE OF TENNESSEE

7-23-98

CORRECT Edward F. Wasserman
ENGINEER OF STRUCTURES

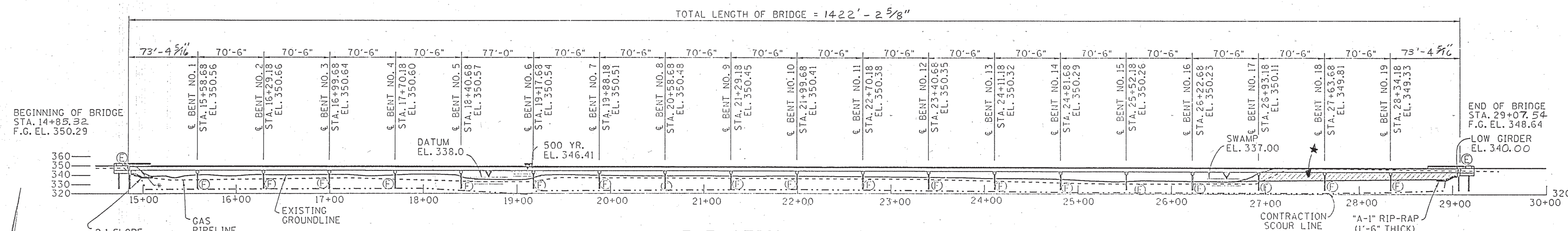
M-369-35

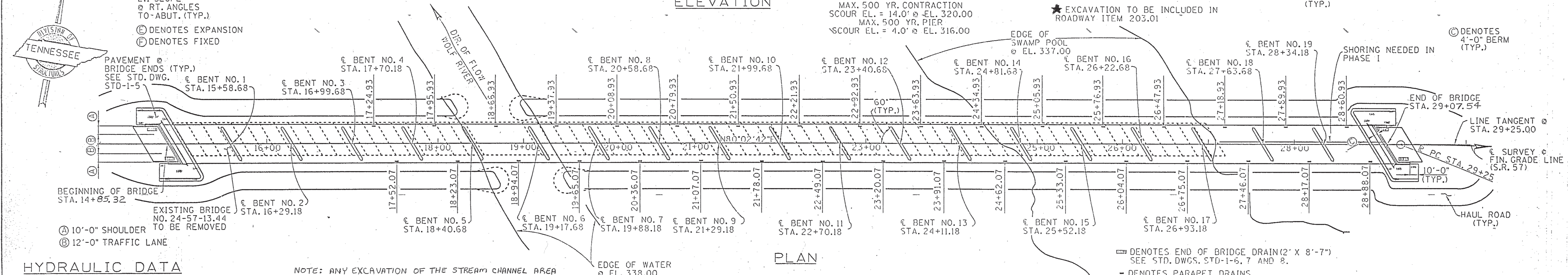
CONST. NO. 24006-3239-94

PROJECT NO.	YEAR	SHEET NO.
BR-STP-57(16)	1997	

REVISIONS

NO.	DATE	BY	BRIEF DESCRIPTION
1	9-2-97	PAM	LAST REVISION DATE
2	9-11-97	PAM	" " "
3	9-15-97	PAM	" " "
4	11-4-97	PAM	" " "
5	4-14-98	PAM	" " "

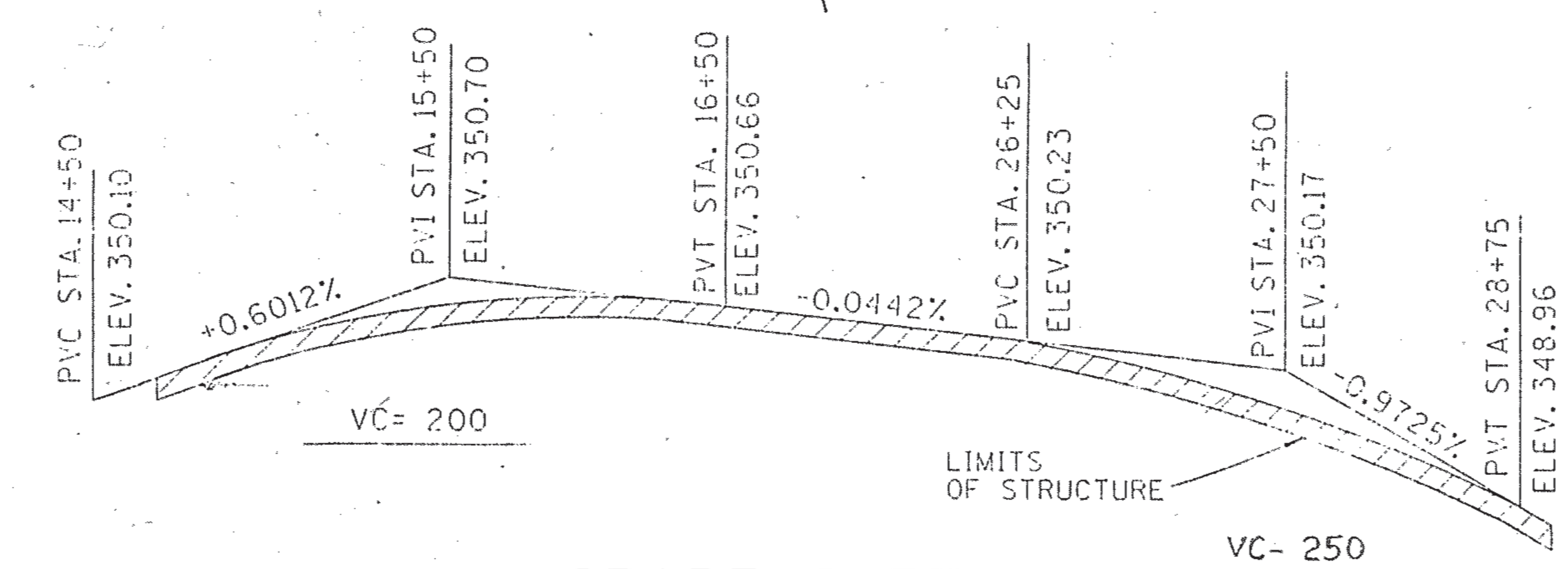




HYDRAULIC DATA

DRAINAGE AREA = 295.7 MI.^{2}
DESIGN DISCHARGE (100 YR.) = 21,040 CFS
WATER AREA PROVIDED BELOW EL. 344.61 = 9971 FT^2
100 YR. VELOCITY = 2.11 FPS
100 YR. BRIDGE BACKWATER = 1.25 FT. @ EL. 345.86
ROADWAY OVERTOPPING EL. = 346.4
500 YR. DISCHARGE 27,990cfs @ EL. 346.41

NOTE: ANY EXCAVATION OF THE STREAM CHANNEL AREA (E.G. FOR RIP-RAP PLACEMENT) SHALL BE ACCOMPLISHED IN THE DRY, DURING LOW-FLOW CONDITIONS. THIS SHALL BE ACCOMPLISHED BY THE USE OF COFFERDAMS, FLUMES, LINED DIVERSION CHANNEL WITH SAND BAG BERM, OR DIVERSION PIPE WITH SAND BAG DAM AT PIPE INLET.



GRADE SKETCH

LIST OF DRAWINGS	DWG. NO.	LAST REV. DATE ①②③④⑤
LAYOUT	m-353-112	4-14-98
GENERAL NOTES	m-353-113	9-15-97
ESTIMATED QUANTITIES	m-353-114	11-4-97
FOUNDATION DATA	m-353-115	
PHASE CONSTRUCTION DETAILS	m-353-116	11-4-97
SUPERSTRUCTURE	m-353-117	11-4-97
SUPERSTRUCTURE DETAILS	m-353-118	
SUPERSTRUCTURE DETAILS	m-353-119	11-4-97
PRESTRESSED I-BEAM DETAILS SPANS 1 THRU 5 & 7 THRU 20	m-353-120	11-4-97
PRESTRESSED I-BEAM DETAILS SPAN 6	m-353-121	
ABUTMENT NO. 1	m-353-122	11-4-97
ABUTMENT NO. 1 DETAILS	m-353-123	
ABUTMENT NO. 2	m-353-124	11-4-97
ABUTMENT NO. 2 DETAILS	m-353-125	9-2-97
PRECAST ALTERNATE BENT No. 1-4, 7-19	M-353-126	4-14-98
" " " DETAILS	M-353-126A	4-14-98
" " " No. 5&6	M-353-127	4-14-98
" " " DETAILS	M-353-127A	4-14-98
FINAL FOUNDATION DATA	m-353-128	
FINAL FOUNDATION DATA	m-353-129	
BILL OF STEEL	m-353-130	11-4-97
BILL OF STEEL	m-353-131	4-14-98

LIST OF SPECIAL PROVISIONS	PROV. NO.	LAST REV. DATE
APPROVAL OF SHOP DRAWINGS	105A	3-06-95
STRIP SEAL EXPANSION JOINTS	604S	2-19-96

LIST OF STANDARD DRAWINGS	DWG. NO.	LAST REV. DATE
▲ BRIDGE RAILING CONCRETE PARAPET	STD-1-1	4-28-97
SLIDER PLATES AND DECK DRAINS	STD-1-2	9-11-95
▲ PAVEMENT AT BRIDGE ENDS	STD-1-5	4-28-97
▲ BRIDGE END DRAIN W/ PABE	STD-1-6	4-28-97
▲ BRIDGE END DRAIN W/ PABE	STD-1-7	4-28-97
BRIDGE END DRAIN 2' X 8'-7" W/ PABE	STD-1-8	5-01-95
STRIP SEAL EXPANSION JOINT	STD-3-1	2-14-94
STRIP SEAL EXPANSION JOINT	STD-3-2	2-14-94
▲ STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-1	4-28-97
STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS DESIGN CRITERIA	STD-4-2	6-10-96
STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS GENERAL DETAILS	STD-4-3	6-10-96
STD. PRECAST, PRESTRESSED BRIDGE DECK PANELS CONSTRUCTION DETAILS	STD-4-4	6-10-96
STD. PILE DETAILS	STD-5-1	10-25-93
STD. PILE DETAILS	STD-5-2	10-26-92
STANDARD SEISMIC DETAILS	STD-6-1	5-01-95
STANDARD SEISMIC DETAILS	STD-6-2	11-07-94
REINFORCING BAR SUPPORT DETAILS FOR CONCRETE SLAB	STD-9-1	12-19-94
MISCELLANEOUS ABUTMENT AND DRAINAGE DETAILS	STD-10-1	5-11-92
STD. DETAILS AND INT. DIAPH. DETAILS FOR I-BEAMS	STD-14-2	NO DATE

▲ DENOTES: THESE DRAWINGS ARE TO BE PRINTED WITH PLANS.

CONST. CURVE DATA

PI	30+37.67
N	282196.74
E	949468.85
Δ	2°28'15.88"RT
D	1°05'48.15"
R	5224.34'
L	225.32'
T	112.68'
E	1.21'
Lc	225.30'
SE	EXIST.

REFERENCE DRAWINGS	DWG. NO.
▲ LAYOUT OF BRIDGE	A-5-166
▲ DETAILS OF PIERS 1 & 2	A-5-167
▲ LAYOUT OF WIDENING	F-14-64
▲ DETAILS OF WIDENING	F-14-65
▲ DETAILS OF WIDENING	F-14-66

WILLIAM R. WOODS — STATE OF TENNESSEE — No. 012289
6-13-97

2017 ADT = 7,070
44'-0" ROADWAY WITH STD-1-1 PARAPET
DESIGN SPEED = 60MPH

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION
LAYOUT
STATE ROUTE 57
OVER
WOLF RIVER
BRIDGE I.D. NO. 24SR0570025
STATION 22+00.00
LOG MILE 13.44
FAYETTE COUNTY
1997

CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

DESIGNED BY PAUL MILLER DATE 04-96
DRAWN BY R. FOSTER DATE 06-97
SUPERVISED BY FIELDS & PARRISH DATE 06-97
CHECKED BY PAUL MILLER DATE 06-97

M-353-112

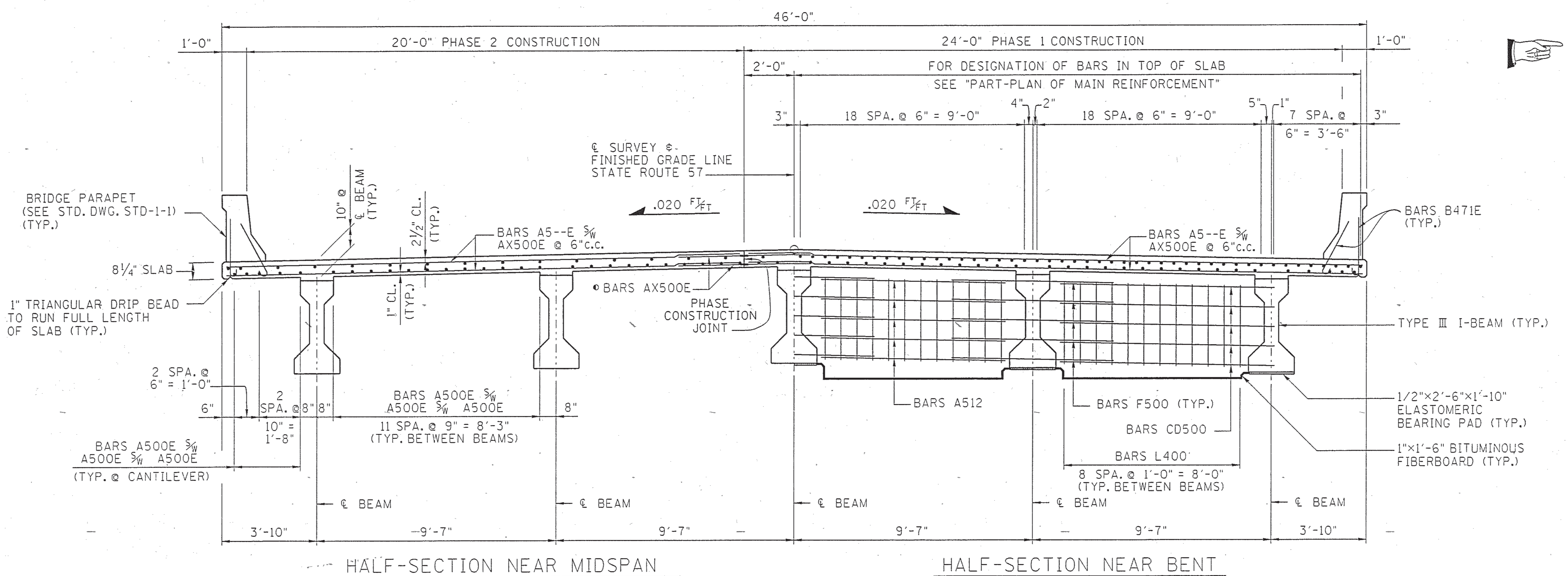
SP142F.DGN

CONST. NO.	24006-3239-94	
PROJECT NO.	YEAR	SHEET NO.
BR-STP-57(16)	1997	

REVISIONS

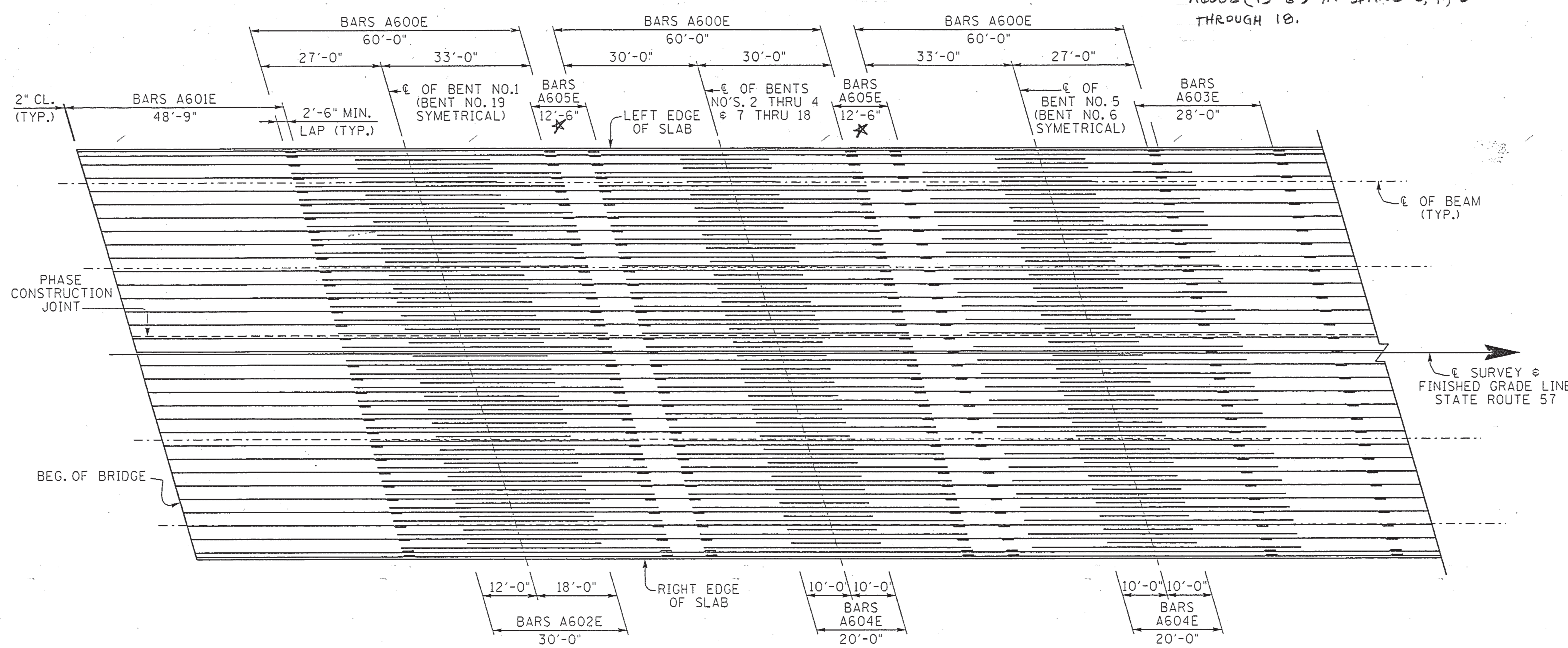
NO.	DATE	BY	BRIEF DESCRIPTION
1	9-2-97	PAM	ADDED NOTE
2	11-4-97	PAM	REVISED QUAN., ADDED NOTE

⚠1 ◐ COUPLER ON BAR AX500E TO BE PAID FOR UNDER ITEM NO. 604-02.03.



TYPICAL CROSS SECTION
(LOOKING FORWARD ON SURVEY)

⚠2 *NOTE: USE BARS A605E (12'-6") IN SPANS 2, 5, 7, AND 9. USE BARS A606E (15'-6") IN SPANS 3, 4, 8 THROUGH 18.



PART-PLAN OF MAIN REINFORCEMENT

SUPERSTRUCTURE GENERAL NOTES:

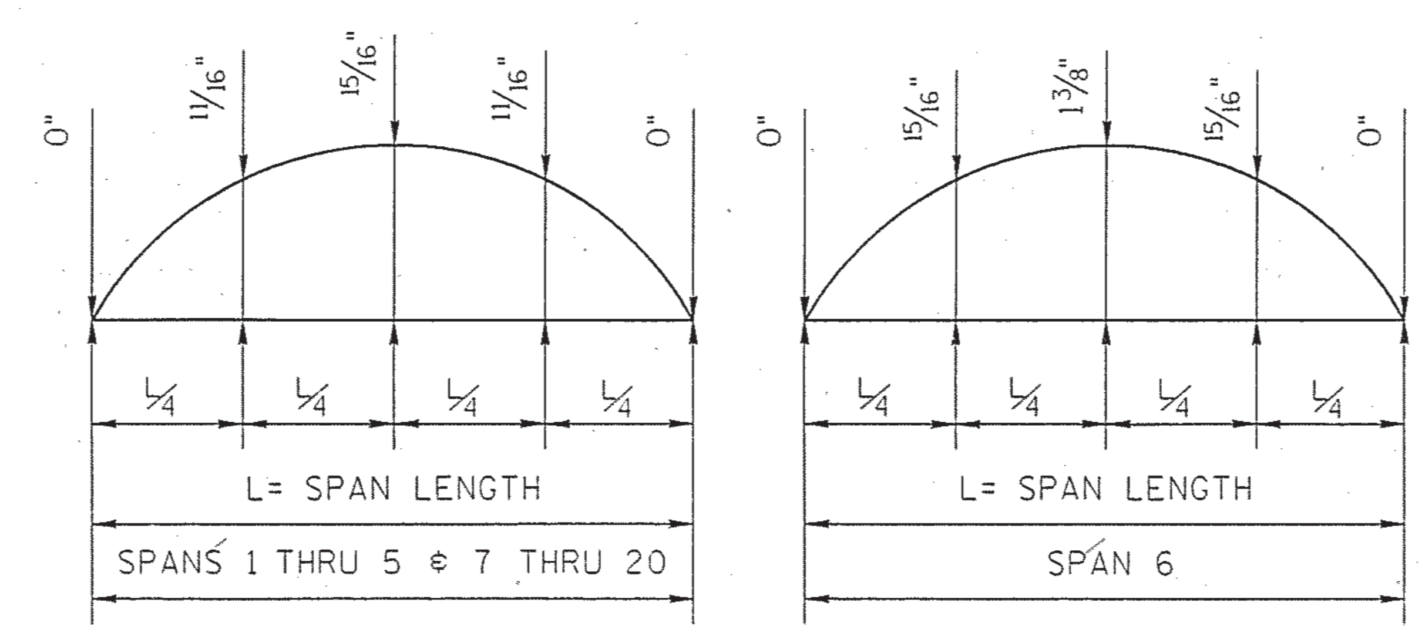
NOTE: NO PORTION OF THE PARAPET SHALL BE POURED UNTIL THE ENTIRE DECK SLAB IS IN PLACE.

SPECIAL NOTE FOR ANCHOR BOLTS AT PIERS: ANCHOR BOLT ASSEMBLIES AT PIERS SHALL BE IN ACCORDANCE WITH STANDARD DRAWING STD-6-1.

NOTE: WHEN POURING SLAB, PROVISIONS SHALL BE MADE FOR SETTING REINFORCING STEEL FOR PARAPET. THE PARAPET SHALL NOT BE POURED UNTIL THE SLAB IS POURED AND CURED. ALSO, SEE STANDARD DRAWING STD-1-1.

NOTE: THE CONTRACTOR IS SOLELY RESPONSIBLE FOR SUPPORTING THE BEAMS TO PREVENT DAMAGE DUE TO TWISTING OR OVERTURNING DURING ALL PHASES OF CONSTRUCTION. IT IS STRONGLY RECOMMENDED THAT THE TEMPORARY ERECTION DIAPHRAGMS BE INSTALLED AND CURED PRIOR TO PLACING ANY LOADS ON THE GIRDERS. HOWEVER, TEMPORARY ERECTION DIAPHRAGMS MUST BE IN PLACE IN THE SPAN AT THE TIME THE SLAB IS POURED IN SAID SPAN.

NOTE: SUPPORT DIAPHRAGMS AT BENTS AND END DIAPHRAGMS AT EXPANSION JOINTS SHALL BE POURED CONCURRENTLY WITH THE DECK SLAB AND INCLUDED IN THE QUANTITY FOR ITEM 604-03.09.



DEAD LOAD CORRECTION CURVE

THIS CURVE IS FOR DEAD LOAD SLAB AND ALL DEAD LOADS THAT ARE APPLIED AFTER THE SLAB IS IN PLACE AND SHOULD BE CORRECTED TO COMPENSATE FOR THE EFFECTS DUE TO VERTICAL CURVE.

IF PRESTRESSED DECK PANELS ARE USED AND THE BEAMS ARE PROFILED AFTER PANELS ARE IN PLACE, REDUCE THE DEAD LOAD CORRECTION VALUES SHOWN BY 25%.

⚠2 ESTIMATED QUANTITIES

CONCRETE CLASS "D" (BRIDGE DECK) C.Y.	EPOXY COATED REINFORCING STEEL LB.	REINFORCING STEEL L.B.
1952	551,750	18950

STATE OF TENNESSEE
DEPARTMENT OF TRANSPORTATION

SUPERSTRUCTURE
STATE ROUTE 57
OVER
WOLF RIVER
STATION 22+00.00
FAYETTE COUNTY
1997

WILLIAM R. WOODS — STATE OF TENNESSEE (seal)

9-2-97
CORRECT Edward P. Wasserman
ENGINEER OF STRUCTURES

DESIGNED BY	PAUL A. MILLER	DATE	04-96
DRAWN BY	ROGER FOSTER	DATE	05-97
SUPERVISED BY	FIELDS & PARRISH	DATE	05-97
CHECKED BY	PAUL A. MILLER	DATE	06-97

M-353-117